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AGENDA

Congestion Management & Environmental Quality (CMEQ)

Committee

Date: Monday, June 26, 2017
Time: 3:00 p.m.
Place: San Mateo City Hall
330 West 20th Avenue, San Mateo, California
Conference Room C (across from Council Chambers)

PLEASE CALL Jeff Lacap (650-599-1455) IF YOU ARE UNABLE TO ATTEND

- | | | |
|--|-------------------------------------|--------------|
| 1. Public comment on items not on the agenda. | Presentations are limited to 3 mins | |
| 2. Issues from the May and June 2017 C/CAG Board meeting: <ul style="list-style-type: none">• Approved – Appointments of Shelly Masur (Redwood City), Peter Ratto (SamTrans), and Dave Pine (JPB) to the CMEQ Committee.• Approved – OBAG 2 LS&R Rehab Program and FAS list of projects and augmentation of planning funds for submission to MTC.• Approved – Updated San Mateo County Priority Development Area (PDA) Investment & Growth Strategy. | Information (Lacap) | No Materials |
| 3. Approval of minutes of April 24, 2017 meeting. | Action (Garbarino) | Pages 1 - 4 |
| 4. Receive a presentation on the US 101 Managed Lanes Project. | Information (Scott) | No Materials |
| 5. Review and recommend approval of the C/CAG Countywide Carpooling Incentives Pilot Program and partnership with Scoop and Waze. | Action (Hoang) | Pages 5 – 8 |
| 6. Review and recommend approval of the draft call for projects for the Safe Routes to School and Green Streets Infrastructure Pilot Program. | Action (Bogert) | Pages 9 – 32 |
| 7. Receive an update on Regional Measure 3. | Information (Wong) | No Materials |



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- | | | |
|---|----------------------------|--------------|
| 8. Executive Director Report. | Information (Wong) | No Materials |
| 9. Member comments and announcements. | Information
(Garbarino) | |
| 10. Adjournment and establishment of next meeting date:
August 28, 2017 | Action (Garbarino) | |

NOTE: All items appearing on the agenda are subject to action by the Committee. Actions recommended by staff are subject to change by the Committee.

NOTE: Persons with disabilities who require auxiliary aids or services in attending and participating in this meeting should contact Mima Guilles at 650 599-1406, five working days prior to the meeting date.

Other enclosures/Correspondence - None

**CITY/COUNTY ASSOCIATION OF GOVERNMENTS COMMITTEE ON CONGESTION
MANAGEMENT AND ENVIRONMENTAL QUALITY (CMEQ)**

**MINUTES
MEETING OF April 24, 2017**

The meeting was called to order by Chair Garbarino in Conference Room C at City Hall of San Mateo at 3:00 p.m. Attendance sheet is attached.

1. Public comment on items not on the agenda.

None.

2. Issues from the March and April 2017 C/CAG Board meeting.

C/CAG Staff Jeff Lacap provided updates on items that were previously brought to the CMEQ committee and been brought to the Board meeting thereafter.

3. Approval of minutes of February 27, 2017 meeting (Action).

*Motion: To approve the minutes of the February 27, 2017 meeting, O'Connell/Beach.
Motion passes unanimously.*

4. Receive a list of "next-steps" items post-adoption of the San Mateo Countywide Transportation Plan 2040 (SMCTP 2040) (Information).

C/CAG Staff John Hoang presented a list of follow up action items to the San Mateo Countywide Transportation Plan 2040. These items include editorial changes to the text, engaging with stakeholders, and revisiting the Equity Analysis and performance measures.

5. Review and recommend approval of the One Bay Area Grant 2 (OBAG 2) Local Street and Roads Rehabilitation Program and Federal-Aid Secondary (FAS) list of projects and augmentation of planning funds for submission to Metropolitan Transportation Commission (MTC) (Action).

C/CAG Executive Director Sandy Wong presented this item for C/CAG Staff Member Jean Higaki. On March 9, 2017 the C/CAG Board approved the One Bay Area Grant 2 (OBAG 2) Transportation for Livable Communities (TLC) and Bicycle and Pedestrian Improvement Program (BPIP) list of projects for submission to MTC. Approximately \$807,000 remains after funding all eligible TLC and BPIP projects. The CMEQ Committee recommended that the Board direct undersubscribed funds towards the Local Streets and Roads Rehabilitation Preservation (LS&R) Program at the February 27, 2017 CMEQ meeting.

C/CAG Staff is proposing to direct \$557,000 in remaining funds to augment the LS&R program and proposing to direct \$250,000 to augment planning and outreach activities. The Congestion Management Program Technical Advisory Committee (TAC) recommended approval of that proposal at their April 20, 2017 meeting. Committee members asked about the road mile/population based formula used to distribute LS&R funds to jurisdictions and the funds proposed to C/CAG Staff for planning and outreach activities. The Committee asked staff to verify the LS&R formula as a caveat to approving this item.

Motion: To recommend approval of the One Bay Area Grant 2 (OBAG 2) Local Street and Roads Rehabilitation Program and Federal-Aid Secondary (FAS) list of projects and augmentation of planning funds for submission to Metropolitan Transportation Commission (MTC), O'Connell/Bonilla. Beach, Aguirre, O'Neill, Garbarino, Powell, Lee, Keener, Levin, and Koelling approved. Lewis opposed.

6. Review and recommend approval of the updated San Mateo County Priority Development Area (PDA) Investment & Growth Strategy (Action).

C/CAG Executive Director Sandy Wong presented the update to the San Mateo County Priority Development Area (PDA) Investment & Growth Strategy. Sandy introduced Jeff Baird, from Baird + Driskell Community Planning, to present the process, requirements, and key highlights of the 2017 update. The last update was done in 2014.

Committee members asked to modify the chart as shown on page 28 of the meeting packet using the same scale for housing units and jobs in order to depict a realistic picture. Committee members also expressed interest in hearing about successful outcomes of affordable housing policies from those jurisdictions that have adopted them.

Motion: To recommend approval of the updated San Mateo County Priority Development Area (PDA) Investment & Growth Strategy, Aguirre/Lee. Beach, Bonilla, O'Connell, O'Neill, Garbarino, Powell, Keener, Levin, and Lewis approved. Koelling opposed.

7. Review and comment on the candidate project list for Regional Measure 3 funding (Information).

C/CAG Executive Director Sandy Wong presented a candidate project list for Regional Measure 3. Revenues from previous Regional Measures went to fund various projects such as infrastructure and seismic improvements to the bridges in the Bay Area, transit improvement projects, and carpool lane projects.

C/CAG has been working with and the San Mateo County Transportation Authority, transit operators, and local jurisdictions in the county to compile the project list that is regionally significant and would benefit toll bridge corridors to submit to MTC. Committee members asked about different funding sources for transportation projects in the region and suggested to add a column to the draft project list showing the estimated cost of each project. Member Levin suggested staff to look into the AC Transit M transbay route crossing the San Mateo Bridge.

8. Executive Director Report (Information).

C/CAG Executive Director Sandy Wong announced the following events: C/CAG Lobby Day will be on May 3rd in Sacramento, MTC Plan Bay Area 2040 Open House will be on May 4th at Sequoia High School in Redwood City, and community meetings for the US-101 Managed Lanes Environmental Phase will be on May 31st at San Mateo City Hall and June 5th at Redwood City Hall.

9. Member comments and announcements (Information).

None.

10. Adjournment and establishment of next meeting date.

The meeting adjourned at 4:29 pm.

The next regular meeting was scheduled for May 22, 2017.

2017 C/CAG Congestion Management & Environmental Quality (CMEQ) Committee Attendance Report													
Agency	Representative	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sept	Oct	Nov	Dec
Metropolitan Transportation Commission	Alicia Aguirre	x			x								
City of Belmont	Charles Stone												
Town of Atherton	Elizabeth Lewis	x	x		x								
City of San Bruno	Irene O'Connell		x		x								
City of Burlingame	Emily Beach	x	x		x								
Environmental Community	Lennie Roberts	x	x										
City of Pacifica	Mike O'Neill	x	x		x								
City of South San Francisco	Richard Garbarino	x	x		x								
Public	Josh Powell	x	x		x								
City of Millbrae	Wayne Lee	x	x		x								
City of San Mateo	Rick Bonilla	x	x		x								
City of Pacifica	John Keener	x	x		x								
Agencies with Transportation Interests	Adina Levin	x	x		x								
Business Community	Linda Koelling	x			x								
Peninsula Corridor Joint Powers Board (Caltrain)	Liz Scanlon												
San Mateo County Transit District (SamTrans)	Doug Kim												

Staff and guests in attendance for the April 24, 2017 meeting:

Sandy Wong, John Hoang, Jeff Lacap - C/CAG Staff

Pete Ratto - SamTrans

David Burruto - San Mateo County BOS

Jeff Baird - Baird and Associates

C/CAG AGENDA REPORT

Date: June 26, 2017

To: Congestion Management and Environmental Quality (CMEQ) Committee

From: John Hoang

Subject: Review and recommend approval of the C/CAG Countywide Carpooling Incentives Pilot Program and partnership with Scoop and Waze

(For further information or response to questions, contact John Hoang at 363-4105)

RECOMMENDATION

Review and recommend approval of the C/CAG Countywide Carpooling Incentives Pilot Program and partnership with Scoop and Waze.

FISCAL IMPACT

Up to \$1,000,000.

SOURCE OF FUNDS

Funding will come from Transportation Fund for Clean Air (TFCA) County Program Manager funds (FY 2017/18) and Congestion Relief Program funds.

BACKGROUND

C/CAG Countywide Carpooling Incentives Pilot Program

The purpose of the proposed C/CAG Countywide Carpooling Incentives Pilot Program (Program) is to encourage commuters and employees of San Mateo County to use carpooling and ridesharing as a sustainable alternative to driving alone when commuting to and from work. Increased usage of carpools and rideshares during peak commute periods reduce single occupancy vehicle travels and results in fewer trips into and out of San Mateo County. Combined with other transportation demand management strategies, the proliferation and sustained utilization of carpools and rideshares contribute towards the decrease of traffic congestion resulting in increased travel time reliability and reduction in greenhouse gas (GHG) emissions within the County.

The Program seeks to utilize emerging technology by partnering with a private sector companies that has developed and specializes in a dynamic carpooling and ridesharing mobile applications (Apps) for real-time ride-matching services for the purpose of supporting implementation of the Program. Subsidies would be provided to commuters (individuals who live or work in San Mateo County) to offset actual cost the commuter incurs for each trip taken utilizing the App, therefore, the service must be able to facilitate C/CAG's financial incentive during the pilot Program. It is the intent that subsidizing rides would significantly increase the number of individuals opting to carpool.

The Program provide benefits to San Mateo County by: reducing the number of single occupancy vehicles during peak commute periods; reducing traffic congestion during peak commute periods, reducing GHG, reduce the need for parking, and to help change long-term driver/commuter behavior.

Similar Implementation by Others

In October 2016, the Cities of San Mateo and Foster City launched concurrent pilot projects with Scoop Technologies (Scoop), developer of the mobile rideshare/carpool matching application “Scoop”, to implement a program that subsidize commuters entering and leaving each respective cities. As part of the pilot program, all Scoop rides to/from these cities would cost the commuter/carpooler \$2 per one-way trip with the cities subsidizing the remaining cost. The standard ride trips can cost anywhere from \$3 to \$12. For the pilot project, the City of San Mateo provided \$30,000 and City of Foster City provided \$60,000. The pilot project would continue until the funds were depleted. In March 2017, Foster City added an additional \$30,000 to extend the project. In addition, in April 2017, the City of South San Francisco also implemented a similar subsidized carpooling program with Scoop.

From a countywide approach, the Contra Costa County Transportation Authority (CCTA) also implemented a similar carpool incentives project with Scoop which began in May 2017.

Partnerships

Based on the successes of the initial pilot projects in San Mateo and Foster City, early this year, C/CAG and our Transportation Demand Management (TDM) partner Commute.org reached out to Scoop with the goal of expanding the individual city model into a countywide pilot program that open services to all cities, including the unincorporated areas of San Mateo County.

As recommended by the CMP TAC at the March 2017 meeting, C/CAG issued a request for proposal (RFP) in May soliciting for companies to submit proposals to provide a dynamic ridematching / carpooling matching application (App) and services for San Mateo County commuters to enable implementation of the Program. C/CAG received two proposals, from Scoop, and from Waze. Based on the two companies’ respective qualifications and the likelihood that the respective App would meet the needs and objectives of the Program in terms of functionality, usability, user-friendliness, features, and other key aspects, staff suggest that C/CAG partner with both companies.

The two project Partners, Scoop and Waze, offers the tool (Apps) that enables C/CAG to provide the carpooling incentives to commuters. The project Partners Apps would help facilitate the process of the subsidizing the carpool trips.

How the Program and App Works

The key aspects of the Program are as follows:

- The incentives will be applied only to trips originating within the peak commute periods of 6:00 a.m. – 9:00 a.m. in the morning and 4:00 p.m. – 7:00 p.m. in the afternoon.
- All user trips in San Mateo County will be reduced by \$2. This includes trips that either begins or ends in San Mateo County.
- A \$2 incentive will be applied through the Apps towards each user trip (rider and driver) in

San Mateo County. Each participant can receive a maximum of \$4 each day (assuming one trip from home to work and a second trip from work to home.)

- Individuals eligible to receive the incentives include residents of San Mateo County and employees who work in San Mateo County.
- Scoop, Waze, C/CAG, Commute.org and San Mateo County cities and the County to jointly market and promote services.
- The Program rollout is planned for July 2017.
- The Program will offer up to \$1,000,000 to subsidize the cost of the carpool program (including reimbursement for guarantee ride home expense for San Mateo County commuters) over a one (1) year period or until funds are exhausted.

Aside from the above Program elements, the responsibilities of the Partners (Scoop and Waze) include:

- In addition to industry best practices, Partners are required to implement a Motor Vehicle History check on all drivers participating in the Program
- Partners will work with C/CAG to implement a Guarantee Ride Home element as part of the carpooling services.
- Partners will provide monthly reports to C/CAG including, but not limited to, information on usage statistics including number active users, number of rides, aggregated original and destination information, aggregated trip lengths, and other reports to be determined that will help C/CAG perform an assessment of the Program.
- Partners will work with Commute.org, to integrate the Apps into the STAR Platform, an online tool that Commute.org uses to promote, track, and incentivize alternative commuting behaviors.

Funding

One of the key purposes of the C/CAG Congestion Relief Plan (CRP) is to develop and fund countywide programs and projects that provide comprehensive benefits in addressing traffic congestion to the overall transportation system in the County. Two of the transportation-related programs the CRP presently funds include the Employer-Based Shuttle and Local Transportation Services and Countywide Travel Demand Management. The Board approved FY 2017/18 CRP budget included funding to implement travel demand management and traffic operational improvement strategies.

The TFCA funds, distributed to C/CAG by the Bay Area Air Quality Management District (BAAQMD) as part of the San Mateo County Program Manager Fund, are intended to be used for projects and programs whose primary objectives include reducing air pollution and GHG emissions and traffic congestion by improving transportation options. At the May 2017 meeting, the C/CAG Board approved the FY 2017/18 Expenditure Plan projects, which included budgeting \$700,000 for the Program.

Utilization of the TFCA portion of budget will be evaluated to ensure that the project meet the established TFCA cost-effectiveness criteria of \$150,000/weighted ton of surplus emission over the project's useful life. Surplus emissions is defined as reactive organic gases (ROG), nitrogen oxides

(NOx), and weighted PM10 (particulate matter 10 microns in diameter and smaller. Therefore, it is proposed that the Program have the flexibility to adjust contributions from CRP and TFCA funds, as needed, to optimize each funding sources, based on staff evaluation.

Program Evaluation

It is intended that the Pilot Program will be evaluated monthly to determine commuter behavior and analyze trends by monitoring ridership, origins and destinations of the trips (aggregated), time of day, number of carpoolers, and other data that is made available by the respective Partners. Adjustments to Program will be made, as applicable, during the pilot period. At the conclusion of the Program, a full assessment will be performed.

ATTACHMENTS

None.

C/CAG AGENDA REPORT

Date: June 26, 2017

To: C/CAG Congestion Management and Environmental Quality Committee (CMEQ)

From: Reid Bogert

Subject: Review and recommend approval of draft call for projects for the Safe Routes to School and Green Streets Infrastructure Pilot Program

(For further information or questions, contact Reid Bogert at 650-599-1433)

RECOMMENDATION

Review and recommend approval of draft call for projects for the Safe Routes to School and Green Streets Infrastructure Pilot Program

FISCAL IMPACT

Up to \$2 million.

SOURCE OF FUNDS

Funded in equal parts by local \$4 vehicle license fees (AB 1546) designated for regional stormwater pollution prevention programs and \$10 vehicle license fees (Measure M) designated for Safe Routes to School programs.

BACKGROUND

C/CAG staff is developing a call for projects for the Safe Routes to School (SRTS) and Green Streets Infrastructure Pilot Program (Pilot Program), which is intended to fund integrated improvements within the public right-of-way that increase safety for children walking and biking to school, while also improving water quality, increasing urban greening, and enhancing the pedestrian environment. A primary goal of the Pilot Program is to demonstrate that green infrastructure and pedestrian improvements can be cost-effectively integrated to increase safety and achieve stormwater goals for C/CAG's member agencies. The Pilot Program is intended to fund combinations of vegetated curb extensions and pedestrian bulbouts/curb ramps with crossings/striping at intersections or mid-block crossings near schools.

C/CAG will provide up to \$2 million in equal shares of local vehicle license fees designated for SRTS and stormwater pollution prevention to fund the Pilot Program. The Pilot Program is designed to provide grant funds with equal shares of SRTS and stormwater funds; as such, proposed projects must include balanced combinations of SRTS and stormwater features. Funds are available to C/CAG member agencies, and each agency may apply for a minimum of \$100,000 and a maximum of \$250,000 per project, with a two project limit per jurisdiction. If applying for funding for two projects, the proposed individual projects should be geographically separate or otherwise functionally distinct. Funds are for construction projects and costs only (planning, design, or staff time is not eligible for funding). There is a 15% local cash match requirement to further leverage C/CAG's funding.

Project locations must either be identified in an existing Walk Audit or Comprehensive Bicycle and Pedestrian Plan, and should directly benefit children walking or biking to/from a nearby public or private school. If not identified in a local Walk Audit or Bicycle/Pedestrian Plan, projects should occur within ½ mile of a school and provide reasonable justification for eligibility (e.g., accident statistics or other supporting data). The Pilot Program will not fund non-infrastructure projects, or ineligible project elements, including pedestrian or street lighting, illuminated crosswalks, or rectangular rapid flashing beacons (RRFB), which would potentially create an unbalanced cost distribution between stormwater and pedestrian features.

C/CAG and San Mateo County Office of Education staff co-hosted a coordination meeting on May 18, at which municipal representatives were paired with SRTS coordinators to learn about the planned solicitation and discuss potential project opportunities. C/CAG staff also plans to hold a pre-application workshop during the solicitation period to address specific questions from interested parties.

SRTS/Green Streets Infrastructure Project Schedule (tentative):

Event	Date
Call for Projects Issued	Mid-July, 2017
Applications Due	Friday, September 15, 2017
Selection Panel Reviews Applications	End of September, 2017
C/CAG Committees Review Selection Panel Recommendations	Thursday, October 19, 2017
C/CAG Board Considers Recommendations	Thursday, November 9, 2017
Execute Funding Agreements with Project Sponsors for Awarded Projects	Thursday, March 01, 2018
Construction Complete	Monday, October 01, 2019
Final Reimbursement Requests Due	Monday, December 31, 2019

ATTACHMENTS

1. Draft Safe Routes to School and Green Streets Infrastructure Call for Projects
2. Draft Safe Routes to School and Green Streets Infrastructure Application

ATTACHMENT 1 - Draft Safe Routes to School and Green Streets Infrastructure Call for Projects

Safe Routes to School (SRTS) and Green Streets Infrastructure Pilot Program Funding Guidelines

Background

The City/County Association of Governments of San Mateo County (“C/CAG”) is a joint powers agency whose members are the County and the 20 cities and towns in San Mateo County. Its primary role is a Congestion Management Agency, but it has also administered the Countywide Water Pollution Prevention Program since its inception in the early 1990s, with a primary goal of assisting member agencies in meeting municipal stormwater regulatory mandates.

The San Mateo County Safe Routes to School Program (“SRTS”) is a partnership between C/CAG and the San Mateo County Office of Education (“SMCOE”), the goal of which is to encourage and enable school children to walk and bicycle safely to school and reduce congestion and emissions caused by school related travel. The program primarily focuses on non-infrastructure projects and activities to improve health and safety and reduce traffic congestion.

Project Call

The Safe Routes to School and Green Streets Infrastructure Pilot Program (“Pilot Program”) is intended to fund integrated improvements within the public right-of-way that increase safety for children walking and biking to school, while also improving water quality, increasing urban greening, and enhancing the pedestrian environment. The focus of the Pilot Program is integrated improvements at intersections or mid-block crossings, all within the public right-of-way.

A primary goal of the Pilot Program is to demonstrate that green infrastructure can be cost-effectively integrated with traditional Safe Routes to School infrastructure projects to enhance safety and to achieve stormwater pollutant load reductions for C/CAG’s member agencies, in accordance with the Municipal Regional Stormwater Permit (MRP). C/CAG will provide up to \$2.0 million to fund the Pilot Program through combining equal amounts of funding designated for SRTS and stormwater management. Funding is provided from local vehicle license fees designated for congestion management and pollution prevention. Applicants are eligible to apply for multiple project locations, but awards will be limited to two project locations per applicant, with a maximum grant award of \$250,000 per project location.

Eligible Applicants

Only local governments (cities, towns, and the County) in San Mateo County are eligible applicants for funding through the program. Although a local agency may choose to collaborate with a school district to design, build, or maintain a proposed project, the applying agency will be responsible for project delivery and ensuring sustained implementation of an operations and maintenance plan.

Eligible Project Locations

Proposed project locations should have been previously mentioned in a Walk Audit or Comprehensive Bicycle and Pedestrian Plan, and should directly benefit children walking or biking to a public or private school. If not mentioned in a previous Walk Audit or Pedestrian Bike Plan, the proposed project must be within a ½ mile radius of a school and other reasonable justification should be provided, such as accident statistics or other data as to why the particular location is deemed appropriate.

The pilot program seeks to fund projects entirely in the public right-of-way; however, proposed projects that occur primarily in the right-of-way, but have minimal connection with school property (e.g., a sidewalk connecting to a curb ramp, bulb out or vegetated curb extension) and still adhere to the goals of integrating SRTS and green infrastructure, may be considered on a case by case basis.

Individual project locations, eligible for up to \$250,000 each with a limit of two projects per applying jurisdiction, should be geographically separate or otherwise functionally distinct so as to be designated as individual projects. If applying for funding for two project locations (such as two intersections in close proximity), the local agency should provide sufficient information regarding the designation as separate projects.

Eligible Activities

This pilot program will fund infrastructure projects only (i.e., planning, outreach, and other non-infrastructure projects are ineligible for funding), and only construction capital costs are eligible for funding. Any staff time and overhead costs are not eligible for reimbursement. See **Funding Details** below for information on match requirements and eligible costs.

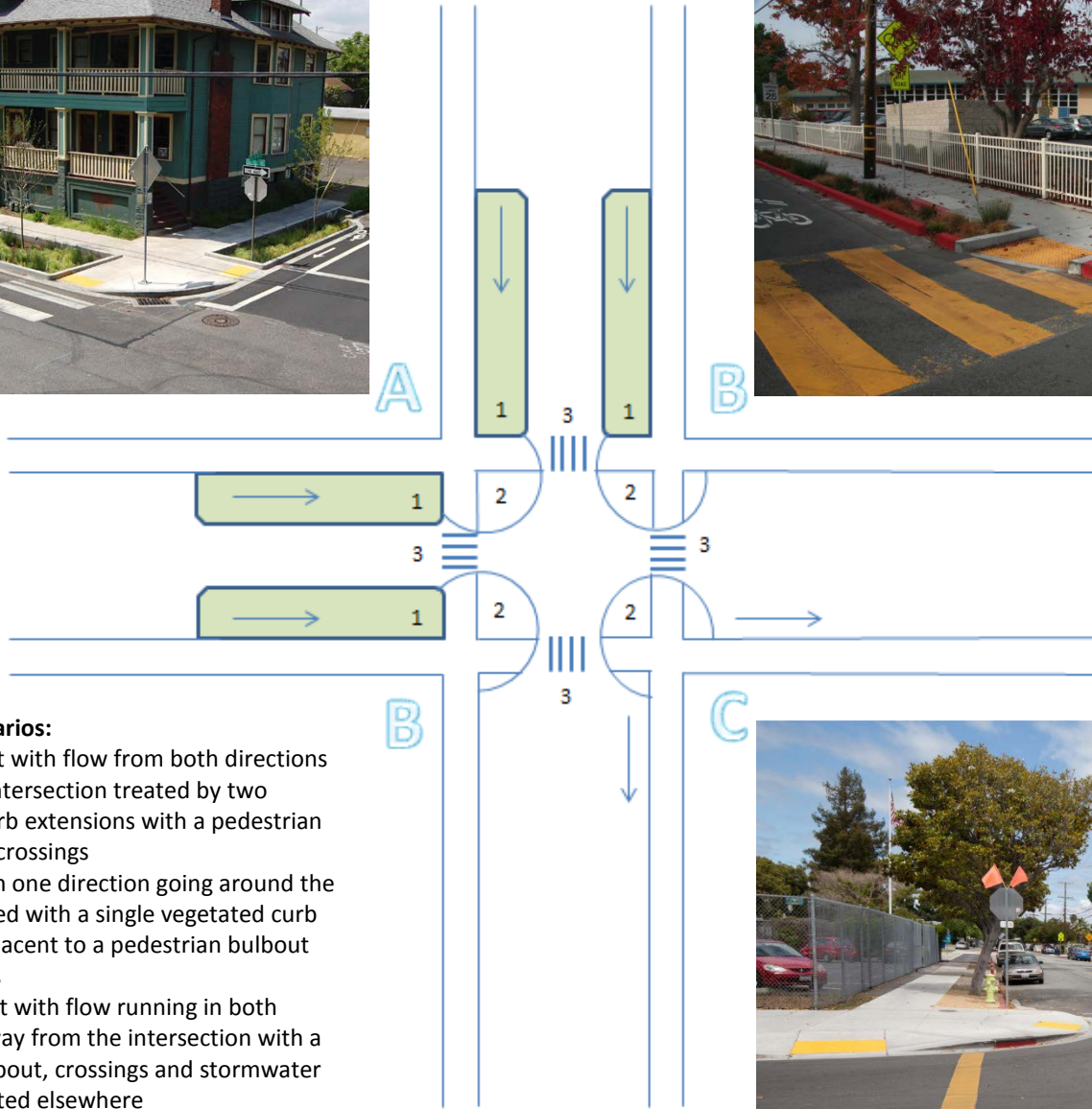
Eligible Project Elements

The intention of the pilot program is to fund integrated stormwater management and SRTS projects in the right of way. Therefore, eligible project elements should include a balanced combination of vegetated curb extensions with pedestrian enhancements at intersections or mid-block crossings. C/CAG staff has created a series of potential scenarios occurring at a model intersection and mid-block crossing, shown in Figures 1 and 2 below. These scenarios demonstrate the intention of this pilot program and should help guide development of project proposals.

Eligible project elements could include:

- Vegetated curb extensions (1)
- Pedestrian bulb outs/curb ramps (2)
- Pedestrian striping/crosswalks (3)

Scenarios



Project Scenarios:

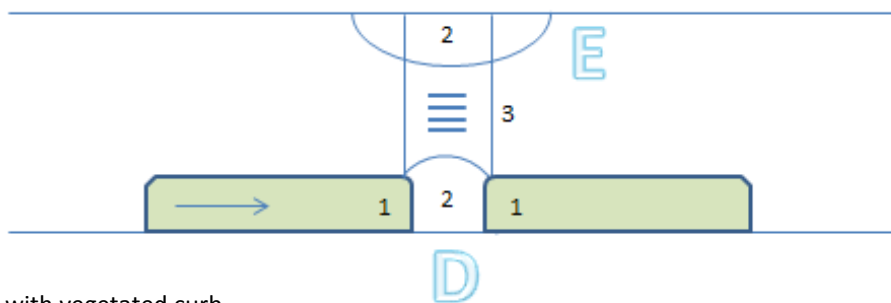
A – Low point with flow from both directions toward the intersection treated by two vegetated curb extensions with a pedestrian bulbout and crossings

B – Flow from one direction going around the corner, treated with a single vegetated curb extension adjacent to a pedestrian bulbout and crossings

C – High point with flow running in both directions away from the intersection with a standard bulbout, crossings and stormwater features located elsewhere



Figure 1. Diagram of eligible project elements at an example four-way intersection



Project Scenarios:

D – Mid-block crossing with vegetated curb extensions on either or both side(s) of crossing

E – Mid-block crossing with a standard bulbout and stormwater features located elsewhere

Figure 2. Diagram of eligible project elements at an example mid-block crossing

Project elements can include any individual or combination of scenario(s) A-E in Figures 1 and 2. These are generalized conditions and are meant to guide applicants toward identifying eligible project components. Site conditions and intersection retrofits will vary; however, **all proposed projects should demonstrate an equal balance among stormwater and SRTS improvements** using the eligible project elements (1, 2, 3). For example, if an intersection improvement project only includes standard pedestrian bulbouts (e.g., no vegetated curb extensions to manage stormwater, as in scenarios C and E), due to specific site conditions and direction of stormwater flow, an additional scenario would need to be included that manages stormwater (e.g., scenarios A, B, or D) and C or D and C). Projects need not be constructed at four-way intersections. The pictures associated with each scenario are examples of what would be considered eligible project designs, and more example projects throughout San Mateo County are provided in the **Resources** section below.

In regard to sizing vegetated curb extensions, C/CAG is seeking to fund projects that will help in reducing pollutant loads, as required under the Municipal Regional Permit. As such, project proposals should include sizing details for the green infrastructure features. Project applicants should indicate whether the proposed green infrastructure elements meet the 4% “rule of thumb” sizing (treatment area to tributary drainage area), or have been sized more efficiently in accordance with the Countywide Water Pollution Prevention Program’s C.3 Technical Guidance manual. Proposals should delineate tributary drainage areas, stormwater flow direction, and locations of existing storm drain inlets in the project vicinity.

Project proposals should generally show a balance between SRTS and green infrastructure features and associated costs. In order to maintain relative balance between SRTS and stormwater costs within projects, the following pedestrian and green infrastructure enhancements will not be funded. Project proposals may include these elements, but the associated costs of these elements will not be reimbursed, and project applicants will need to identify these costs separately in the proposed budget.

This pilot program will NOT fund the following pedestrian enhancements:

- Pedestrian or Street Lighting
- Rectangular Rapid Flash Beacon (RRFB)
- Illuminated Crosswalks
- Raised Crosswalks

This pilot program will NOT fund the following stormwater features:

- Landscaping elements that are not designed to capture and manage stormwater, unless proposed as part of a pedestrian bulb out that is not suitable for stormwater management due to flow direction or other constraints
- Porous pavement/asphalt/concrete in-lieu of vegetated curb extensions/bulb outs

Funding Details

There is a total of up to \$2.0 million dollars available under the current call for projects. Grant funds may only be used to fund construction costs. A minimum of \$100,000 and a maximum of \$250,000 will be awarded per project (two project limit per jurisdiction). A local cash match of 15 percent of construction costs is required for program eligibility.

This integrated pilot program is a cost reimbursement program, and all reimbursements will be made after documentation of the completed project is submitted with invoices. Part of the purpose of the pilot program is to determine the relative cost share between SRTS and stormwater elements of integrated projects. Reimbursement requests (including photo documentation of completed projects) should detail final project construction costs, and provide best estimates of the share of costs split between the two programmatic elements. Indirect costs, including any staff time, will not be reimbursed.

The 15 percent local match will be applied to the physical construction costs, and this amount will be taken off of the top of construction costs when reimbursements are requested. Therefore, reimbursements will be 85 percent of physical construction costs, or the full amount of the grant

request, whichever is less. For example, a project that requests \$250,000 through the program, but which has \$275,000 in actual construction costs would be reimbursed \$233,750 at the end of the project. The reimbursed amount is equal to the actual construction cost less the 15 percent local match (\$41,250). If the actual construction costs were at least \$287,500 (which is the \$250,000 maximum per project amount, plus the 15 percent local match), then the agency would be reimbursed the full \$250,000 requested.

All projects must finish construction by October 1, 2019, and final invoices for reimbursement must be submitted by March 1, 2019.

Mandatory Application Elements

All submitted project applications must include a complete application form and the following as attachments or included in the application, as appropriate:

1. A letter of support from the participating school district, explaining how the proposed project will leverage existing SRTS programming or other efforts related to improve walking and biking conditions for children to and from school.
2. A map of the project area showing the project location, the location of the benefitting school(s), and any relevant land use or transportation information. Also include a walking or biking route map to the school, if applicable. Include documentation that proposed location is included in a walk audit or is within a ½ mile of a school with other relevant justification for the appropriateness of the location.
3. A schematic or conceptual design of the proposed SRTS and stormwater infrastructure elements, including the direction of stormwater flow and any relevant street characteristics, including storm drain infrastructure in the project vicinity. The schematic should also include delineations of drainage areas to and treatment capacities for each stormwater feature. Projects will be awarded full points for achieving the Municipal Regional Permit's Provision C.3.d. sizing criteria for the entire drainage area (including estimates for run-on from adjacent parcels) treated by the proposed stormwater features. At a minimum, the proposed features should be sized to treat the drainage area of the street (crown to curb) draining to the feature, using the 4% "rule of thumb" (treatment area to drainage area). More detailed sizing calculations are encouraged using the [SMCWPPP Provision C.3.d. sizing calculation sheet for combination flow and volume based criteria](#), however, as these will better help the selection committee in reviewing proposed projects and will ensure the facilities are appropriately sized.
4. A long-term operations and maintenance (O&M) plan for the stormwater features. The plan should include details (frequency and actions) about specific maintenance activities, including roles and responsibilities, and dedicated funding for the following operations and maintenance components:
 - a. Removal of trash/debris
 - b. Vegetation maintenance
 - c. Erosion control/mulch replacement
 - d. Aesthetics/safety
 - e. Upkeep of overall function of the stormwater features

If there is an agreement between the applying local agency and the benefitting school as to who is responsible for O&M, this should be clearly described in the plan. It should also be recognized that ultimately the applying agency is responsible for ensuring proper long-term maintenance of the stormwater features.

*See [C.3 Technical Guidance Appendix G](#) for recommended O&M actions and an O&M template to maintain green streets stormwater features.

5. A scope of work, project budget, and schedule with specific timelines and tasks for expected deliverables, from design through construction. To the extent feasible, the proposed budget should show the expected cost distribution between the SRTS and green infrastructure components. The actual cost distribution will be reviewed during the reimbursement process, so all final bids and change orders must be submitted with reimbursement requests. The project budget must specify the requested grant amount, which should be calculated as the total estimated construction cost less the 15 percent local match to be applied toward construction. A 10 percent construction contingency cost line item is permitted. See example under **Funding Details**.

Recommended Project Elements

Project proposals will be awarded more points for including the following as attachments or within the application:

1. Community letters of support.
2. Integration of educational signage in the project design.
3. Projects benefitting schools that are participating in the San Mateo County Safe Routes to School (SRTS) Program. If the school is not participating in SRTS, schools may provide evidence of practicing SRTS initiatives (e.g., established in existing plans, participating in community outreach efforts to educate about walking and biking to school).
4. Projects that address localized drainage or flooding issues.
5. Projects that are identified in other local or countywide plans, or that directly support goals, objectives, or projects in other plans, including bike and pedestrian master plans, community-based transportation plans, complete or sustainable streets plans, etc. Applications for projects that provide such benefits to existing plans should identify the relevant plans and describe how the proposed project benefits or is identified in such a plan.
6. Readiness to Proceed – projects that are closer to construction-ready, will be awarded more points in the scoring process.

Resources

Project Examples – The following Google Street View links show infrastructure projects in San Mateo County that demonstrate integrated stormwater and pedestrian improvements at intersections that would qualify for funding by this pilot program (note: some projects show project elements that are ineligible for grant funding, such as flashing beacons and pedestrian-scale lighting).

[Burlingame Ave. and Park Rd., Burlingame](#)

[1651 Hillside Blvd., Colma](#)

[Delaware St. and E 16th St., San Mateo](#)

[Humboldt St. and College Ave., San Mateo](#)

[Mid-block crossing N Humboldt St., San Mateo](#)

[Laurel Elementary School, San Mateo](#)

[Arroyo Ave and El Camino Real, San Carlos](#)

[Old County Rd. and Riverton Dr., San Carlos](#)

Design Guidance – Below is a list of resources for guidance and typical designs, standards and details, as well as operations and maintenance considerations, for green streets stormwater infrastructure.

[C.3 Stormwater Technical Guidance](#)

[San Mateo County Sustainable Green Streets and Parking Lots Design Guidebook](#)

[Bay Area Urban Greening Intersection Retrofit Typical Details \(Report and Conceptual Designs under “Design Charrette” tab at bottom of page\)](#)

[SFPUC Stormwater Design Guidelines, Typical Details and Specifications](#)

[EPA – Elements of a Green Infrastructure Maintenance Business Plan](#)

Evaluation Criteria

Evaluation Criteria	Description	Max Points
Existing Conditions	The project addresses site-specific SRTS and stormwater management needs and demonstrates the benefits of integrating transportation/pedestrian road improvements with green infrastructure for stormwater management.	23
Proposed Project	Project has a well-defined scope of work and timeline identifying the key purpose and objectives.	37
Project Timeline and Budget	Timeline and budget for all phases of project, including information on match requirement (how much is provided and for what construction costs).	10
School and Community Support	Project demonstrates meaningful community support from the benefitting school district, school(s) and other community stakeholders.	30
Total		100

Application Submission

Applicants must submit 5 bound copies and 1 electronic copy of the completed application along with all of the required and supporting documents. All applications must be received at the C/CAG office by **Friday, September 15, 2017 at 5:00 pm**. A workshop for prospective applicants will be held on **XXX**.

Please submit applications to:

Reid Bogert, Stormwater Program Specialist
 C/CAG
 555 County Center, 5th Floor
 Redwood City, CA 94063

C/CAG Safe Routes to School and Green Streets Infrastructure Pilot Schedule (tentative)

Event	Date
Call for Projects Issued	Mid-July, 2017
Applications Due	Friday, September 15, 2017
Selection Panel Reviews Applications	End of September, 2017
C/CAG Committees Review Selection Panel Recommendations	Thursday, October 19, 2017
C/CAG Board Considers Recommendations	Thursday, November 9, 2017
Execute Funding Agreements with Project Sponsors for Awarded Projects	Thursday, March 01, 2018
Construction Complete	Monday, October 01, 2019
Final Reimbursement Requests Due	Monday, December 31, 2019

For any questions regarding the program or application process please contact Reid Bogert at 650-599-1433 or rbogert@smcgov.org.

Scorer:		Project Sponsor:		
Date Completed:		Project Title:		
Evaluation Criteria	Description	Instructions/Scale	Points Assigned	Additional Comments from Scorer
Section II. Existing Conditions	The project addresses site-specific SR2S and stormwater management needs and demonstrates the benefits of integrating transportation/pedestrian road improvements with green infrastructure for stormwater management.	<i>Add points from category A</i>	23	
A. Project Need	1. Project area map showing project location, benefitting school(s) and other relevant land use or transportation information. Walking or biking route map may also be included.	Mandatory No project area map - ineligible	NA	
	2. Project identifies current risks and/or obstacles (physical or perceived) to walking and/or bicycling to and from your school site(s), including for children with disabilities.	<i>Enter a number between 0 and 10</i> No need - 0 Low need - 5 High need - 10	10	
	3. Project identifies localized drainage or flooding issues and/or other community benefits that can be addressed through green infrastructure designs.	<i>Enter a number between 0 and 15</i> No need - 0 Low need - 3 High need - 5	5	
	4. Proposed project locations previously mentioned in a Walk Audit or Ped/Bike Plan, or the project is within 1/2 mile of school AND the sponsor provides reasonable justification.	<i>Enter a number between 0 and 5</i> Poor justification - 0 Satisfactory justification - 3 Strong justification OR project has been mentioned in Walk Audit or Ped/Bike Plan - 5	5	
	5. Project provides direct benefits to an existing local or countywide plan, or directly supports goals, objectives, or projects in other plans, including bike and pedestrian master plans, community-based transportation plans, complete or sustainable streets plans, etc. Evidence is provided identifying the connection between the proposed project and such plans.	<i>Enter a number between 0 and 3</i> Does not provide benefits to an existing plan - 0 May provide some benefit to an existing plan, but lack of evidence - 1 Demonstrates direct benefit to an existing plan and includes strong supporting evidence - 3	3	
Section III. Proposed Project	Project has a well-defined scope of work and timeline identifying the key purpose and objectives.	<i>Add the number of points from categories A-C</i>	37	
A. Project Components	1. Project helps address the problems identified in the Existing Conditions and clearly explains how the infrastructure components will address SRTS and stormwater goals through a balanced and integrated approach, referencing the schematic diagram (scenarios A-D) in the application or providing an alternative schematic and explanation if needed.	<i>Enter a number between 0 and 10</i> Incomplete description/schematic of scenario identified for integrating SRTS and stormwater components - 0 Satisfactory description/schematic of scenario identified for integrating SRTS and stormwater components - 5 Strong scope of work with clearly explained schematic of scenarios - exceeds basic required components in application - 10	10	
	2. Project explains clearly how SRTS and stormwater planning and infrastructure will be integrated. Proposal includes a balanced approach to integration, both in terms of cost distribution and the allocation of project features.	<i>Enter a number between 0 and 3</i> Poorly balanced project components - 0 Adequately balanced project components demonstrated through schematic scenarios or alternative descriptions - 2 Very well balanced project components demonstrated through schematic scenarios or alternative descriptions - 3	3	
	3. Simple design concept of proposed project components, including calculations of treatment capacity for stormwater features and relevant maps delineating drainage areas.	Mandatory - Enter a number between 0 and 10 No design concept - ineligible Poorly developed design concept (lack of detail, missing information) - 2 Satisfactory design concept - 5 Strong design concept (includes excellent detail and planning, along with all calculations for stormwater treatment and associated map delineating drainage areas) - 10	10	

	4. Project includes educational signage to raise awareness about the purpose and value of green infrastructure/SRTS infrastructure	<p><i>Enter a number between 0 and 2</i></p> <p>No educational signage - 0</p> <p>Educational signage included in scope - 2</p>	2	
<i>B. Logistics</i>	1. Does the proposed project/project sponsor have ROW clearance for all property involved?	<p>Mandatory (yes/no)</p> <p>No clearance - ineligible</p>	NA	
<i>C. Operations & Maintenance</i>	1. Proposal identifies a plan for future long-term operations and maintenance of the stormwater features, including a description of necessary maintenance activities, frequency of activities, who will be responsible for O&M, as well as the plan for dedicated funding.	<p>Mandatory - Enter a number between 0 and 10</p> <p>No plan for funding O&M - ineligible</p> <p>Plan for O&M, but no dedicated funding - 2</p> <p>Satisfactory funding plan for O&M with description of activities, frequency and responsibilities - 5</p> <p>Strong O&M funding plan above and beyond expectations to ensure long-term maintenance - 10</p>	10	
<i>D. Project Readiness</i>	1. Project demonstrates readiness to proceed with detailed project concept, budget, timeline.	<p><i>Enter a number between 0 and 2</i></p> <p>Low readiness to proceed - 0</p> <p>Medium readiness to proceed - 1</p> <p>High readiness to proceed - 2</p>	2	
Section IV. Project Timeline and Budget	<i>Timeline and budget for all phases of project, including 15% match requirement contributed to total construction costs. Budgets should also specify cost distributions for SRTS and GI features to extent possible.</i>	<i>Total possible points - 10</i>	10	
<i>A. Timeline and Budget</i>	1. The proposal includes a reasonable project budget and timeline with key dates for all phases of the project, including planning, design and construction. Budget should include 15% match from project sponsor, and may include a 10% contingency applied to construction costs. The proposed budget should also provide a cost distribution for SRTS and GI features and specify the requested grant amount, accounting for the 15% match requirement applied to construction costs.	<p>Mandatory - Enter a number between 0 and 10</p> <p>No budget and timeline - ineligible</p> <p>Weak budget and timeline with missing components - 3</p> <p>Satisfactory budget and timeline without missing components - 5</p> <p>Strong budget and timeline - complete, clearly defined and well-organized - 10</p>	10	
Section V. School and Community Support	<i>Project demonstrates meaningful community support from the benefitting school district, school(s) and other community stakeholders.</i>	<i>Add points from category A</i>	30	
<i>A. School and Community Support</i>	1. In applying for the SRTS/GI application, the highest authorized representative of the applying school district must submit a letter of support	<p>Mandatory - Enter a number between 0 and 15</p> <p>No support - ineligible</p> <p>Weak support - 3</p> <p>Moderate support - 7</p> <p>Strong support - 15</p>	15	
	2. Application includes additional letters of support (LOS) from the community affected by the project.	<p><i>Enter a number between 0 and 10</i></p> <p>No additional LOS - 0</p> <p>Additional letter(s) - medium quality/need - 5</p> <p>Additional letter(s) - high quality/need - 10</p>	10	
	3. The school either participates in the SRTS Program or can show evidence of SRTS initiatives.	<p><i>Optional - Enter a number from 0-5</i></p> <p>Not Participating in SRTS/No Evidence of SRTS Initiatives - 0 points</p> <p>Not Participating In SRTS/Some Evidence of SRTS Initiatives - 2/3 points</p> <p>Participating in SRTS/Substantial Evidence of SRTS Initiatives - 5 points</p>	5	
			100	

ATTACHMENT 2 - Draft Safe Routes to School and Green Streets Infrastructure Application



Safe Routes to School (SRTS) and Green Streets Infrastructure Pilot Program Project Application

Section I: General Project and Applicant Information

General Project Information

Sponsor Agency:

Project Title:

Amount of Funds Requested (\$):

Note: Minimum request is \$100,000 and maximum award is \$250,000 per project location (2 project limit per applying jurisdiction)

Participating School District:

Participating School Name(s) & Address(es):

Contact information of District Safe Routes to School Coordinator:

Project Manager

Name:

Title:

Agency:

Phone Number:

E-mail Address:

Mailing Address:

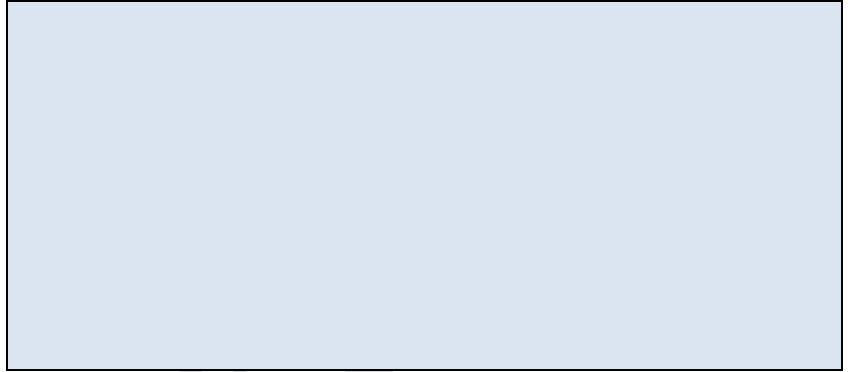
City, State, Zip:

Section II: Existing Conditions

A. Project Need

1. Description of project location and boundaries:

Please attach a map of the project area showing the project location, the location of the benefitting school(s), and any relevant land use or transportation information. Also include a walking or biking route map to the school if applicable.

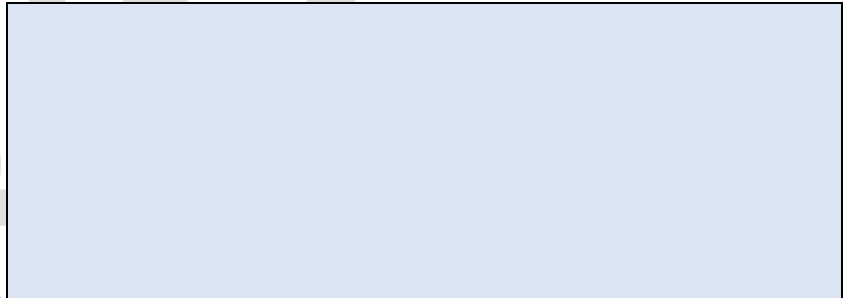


2. Why is the project needed?

What are the current risks and/or obstacles (physical or perceived) to walking and/or bicycling to and from your school site(s), including for children with disabilities?



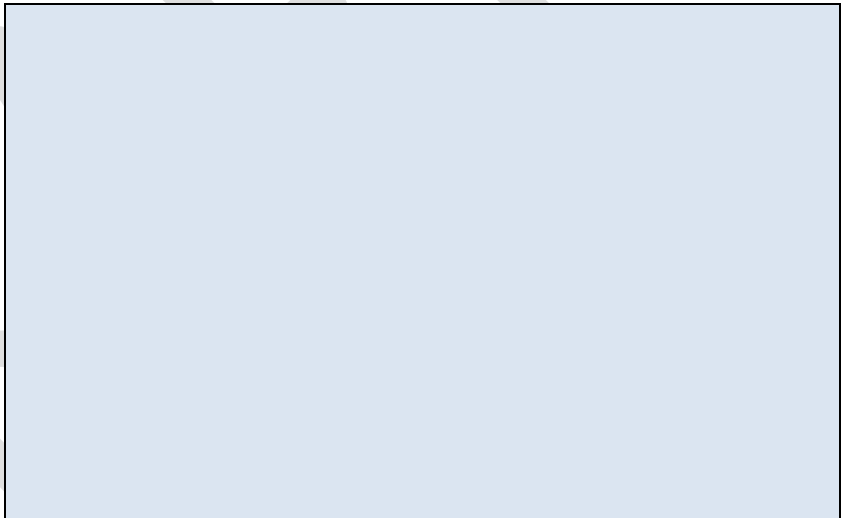
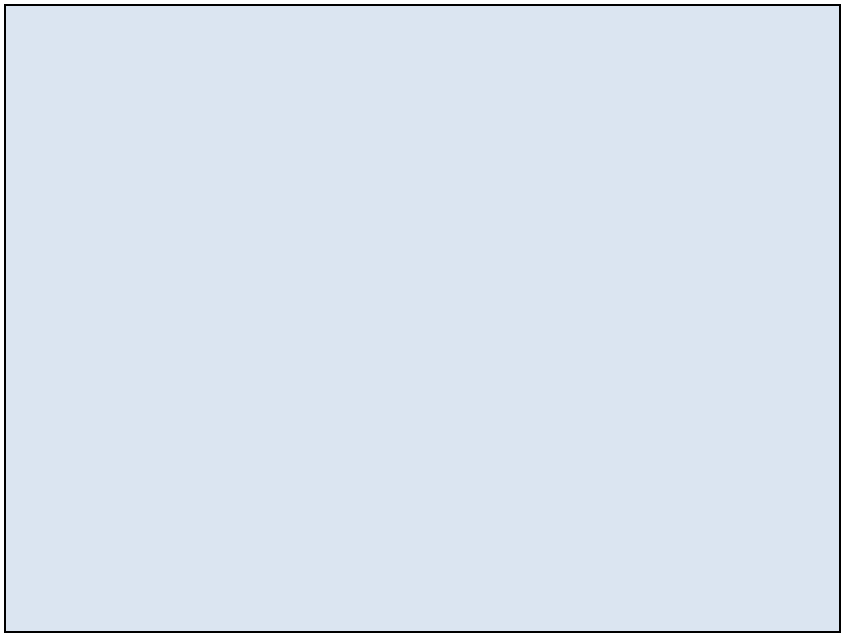
3. How will the project address stormwater management needs at this site and what additional water quality and community benefits will be provided in terms of flow and/or volume capture, flood mitigation, or aesthetic enhancement?



4. Proposed project locations should have been previously mentioned in a city/County Walk Audit or Pedestrian Bike Plan. Include the name of the document and relevant page numbers, as well as an electronic link.

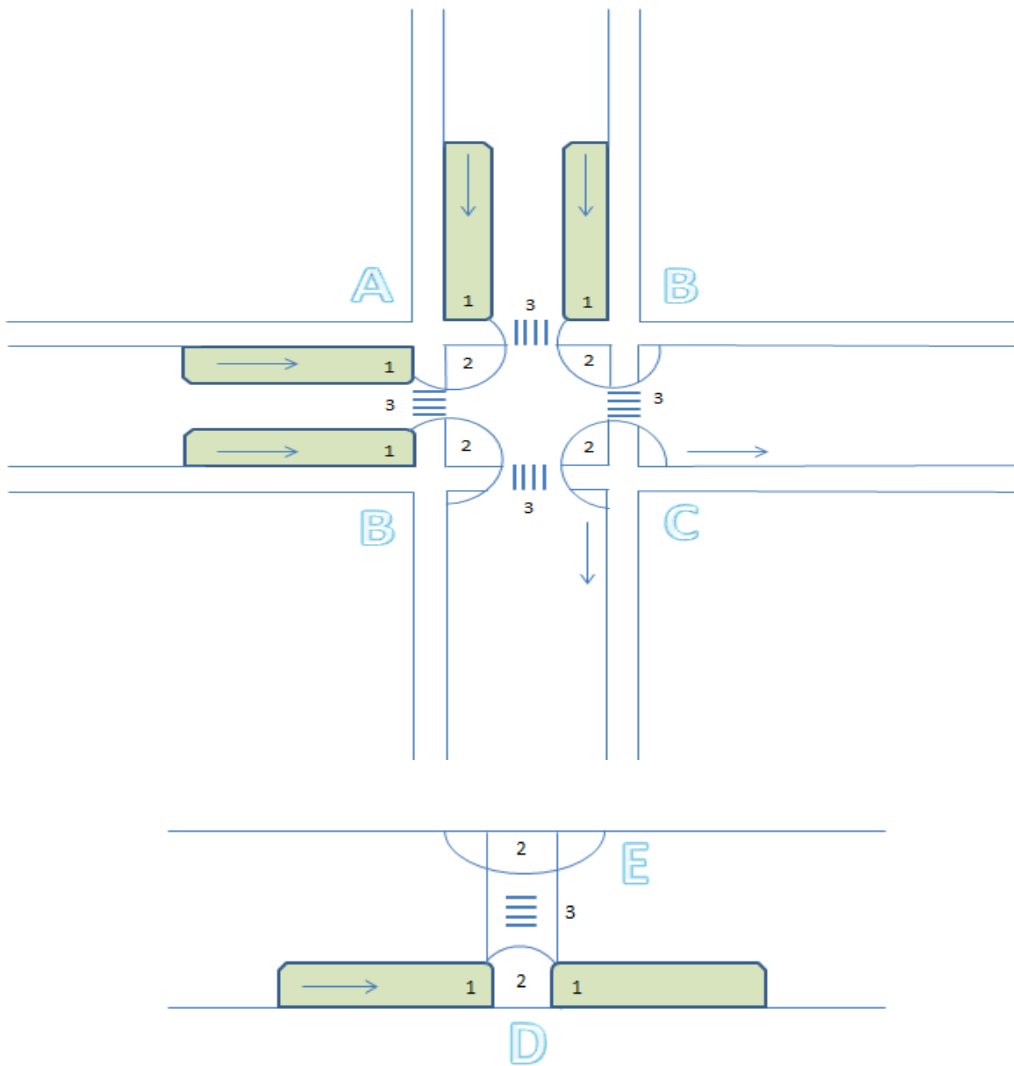
If not mentioned in Walk Audit or Pedestrian Bike Plan, proposed projects must be within a ½ mile radius of school and reasonable justification for the project should be provided, such as accident statistics or other data as to why the particular location is deemed appropriate.

5. Does the proposed project provide direct benefits to an existing local or countywide plan, or directly support goals, objectives, or projects in other plans, including bike and pedestrian master plans, community-based transportation plans, complete or sustainable streets plans, etc.? If so, provide supporting evidence of the connection between the proposed project and such plans.



Provide photos indicating existing conditions and include in your Attachments section.

Section III: Proposed Project



Project Scenarios:

A – Low point with flow from both directions toward the intersection treated by two vegetated curb extensions with a pedestrian bulbout and crossings

B – Flow from one direction going around the corner, treated with a single vegetated curb extension adjacent to a pedestrian bulbout and crossings

C – High point with flow running in both directions away from the intersection with a standard bulbout, crossings and stormwater features located elsewhere

D – Mid-block crossing with vegetated curb extensions on either or both side(s) of crossing

E – Mid-block crossing with a standard bulbout and stormwater features located elsewhere

Legend (Eligible Project Elements):

- Vegetated Curb Extensions (1)
- Pedestrian Bulbouts/Curb Ramps (2)
- Pedestrian Striping/Crosswalks (3)

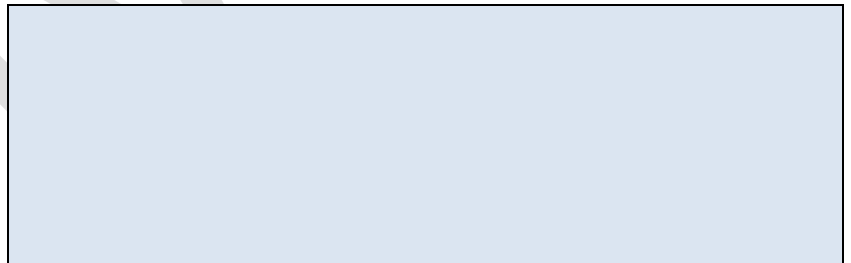
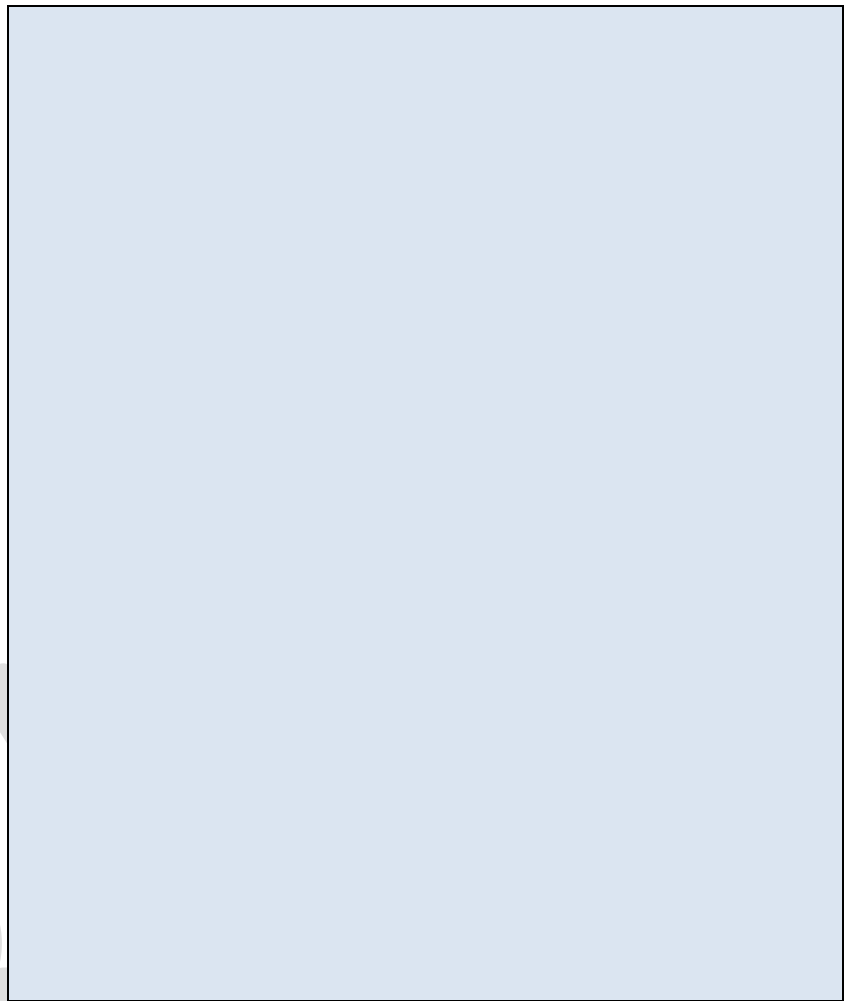
A. Project Components

1. Use the diagram on previous page to explain the proposed SRTS and stormwater infrastructure elements, demonstrating how the proposed components will achieve the SRTS and stormwater goals of this pilot program.

For example, you might describe the proposed project as a combination of scenarios A and D, where you have two vegetated curb extensions and a pedestrian bulbout at a sump condition at one corner of an intersection and a vegetated curb extension and bulbout at a mid-block crossing.

If the project includes an alternative to the general scenarios shown in the diagram above, describe the SRTS and stormwater infrastructure components, illustrating the integration of pedestrian and stormwater infrastructure and indicating the direction of stormwater flow.

2. Explain how the proposed project demonstrates a balanced approach to integrating SRTS improvements with stormwater features, both in terms of estimated costs and allocation of project features in the proposed design.



3. Include as an attachment a simple design concept of all proposed project features. Concepts should include a map delineating the drainage areas for each stormwater feature (either an estimate of the overall drainage area, including adjacent parcels, or at least the crown to curb delineation). At minimum use the 4% of drainage area sizing criteria for calculating the proposed dimensions of the stormwater features. More detailed sizing analysis via the [C.3 Technical Guidance combined flow/ volume sizing calculations](#) is encouraged to optimize sizing and assist the selection committee in evaluating projects. Where the standard C.3.d sizing criteria from the Municipal Regional Stormwater Permit cannot be met, please still include the estimated treatment capacity of the facilities for evaluation. Concepts should also show the location of existing storm drain inlets in the project area.

4. Does the project concept include educational signage to inform the public about stormwater management/SRTS goals?

B. Logistics

1. Do you have Right of Way clearance for all property involved with your project? You must confirm you have the necessary Right of Way in order to receive a grant.

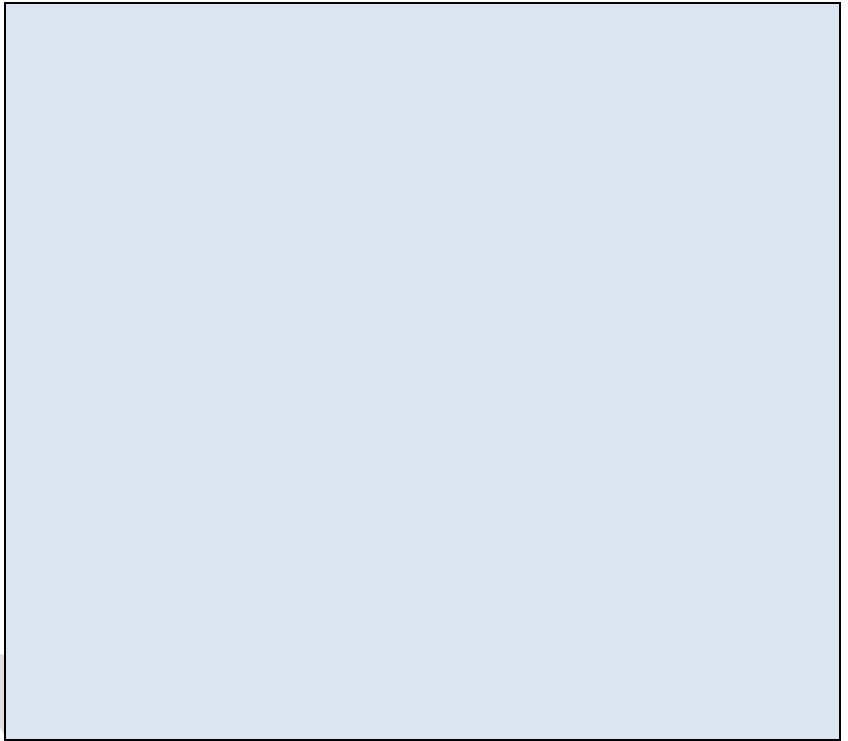
Yes

No

C. Operations and Maintenance

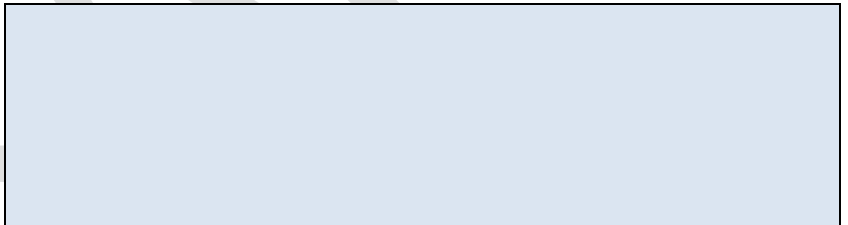
1. Provide a long-term operations and maintenance plan for the completed facilities – identify who will be responsible for long-term operations and maintenance and the dedicated source of funding to ensure sustained operations and maintenance.

Operations and maintenance plans should identify planned maintenance activities and the frequency of these activities, e.g. debris clean-out three times a year; replanting after two years of project completion (if needed), etc. See guidance document for resources. Plans may be included as a separate attachment.



D. Project Readiness

1. Indicate the Readiness to Proceed for the proposed project. Projects that demonstrate a high degree of readiness to proceed will be awarded more points in the scoring process.



Section IV: Project Timeline and Budget

A. Timeline and Budget

1. Please provide a proposed project budget and timeline, including all phases of the project (i.e., planning, design and construction). The budget should include a 15% cash only local match from the project sponsor applied to the total construction cost, so that the requested grant amount is equal to 85% of the total proposed construction cost (see application guidelines for example under Funding Details). All requested and match funds may only be used for eligible project construction costs. The proposed budget may include a 10% contingency for construction. Please include budget and timeline as an attachment to the application.

Section V: School and Community Support

A. School and Community Support

1. Does this project have the support from the participating school?

- Yes – Attach letter of support from school district

No – School district support is mandatory, grant proposals without a letter of support will not be considered

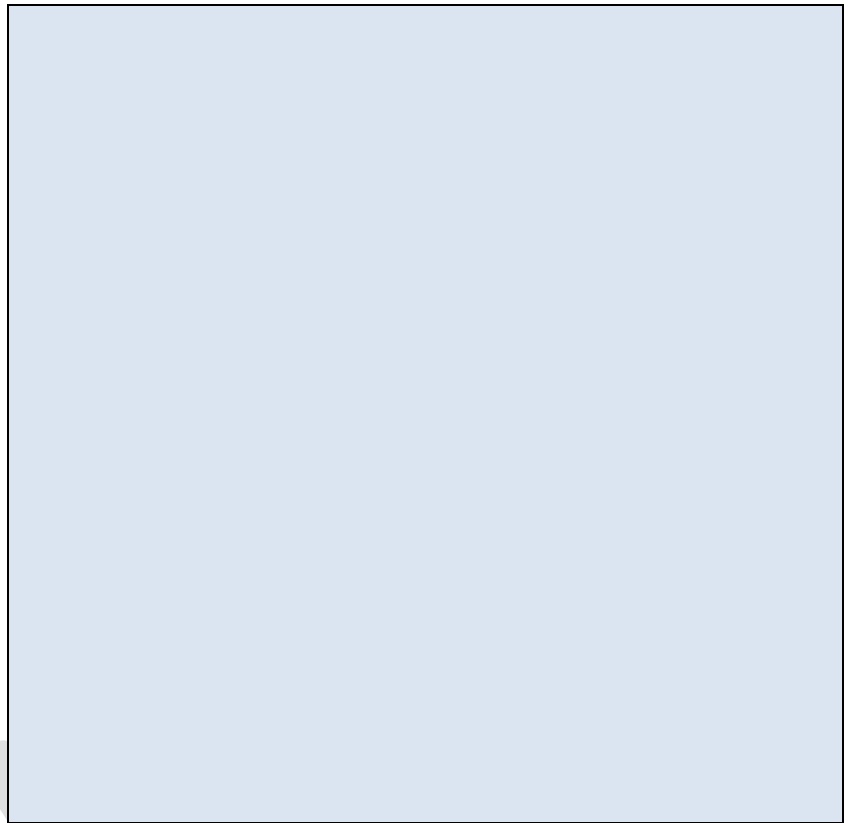
2. Does this project have local community support or involvement?

- Yes – Attach any supporting documentation (e.g. letters of support from local city council, major property owners, neighborhood associations, community groups, transit operators, etc.)

No

3. Describe any existing programs at the participating school(s) (including SRTS initiatives) that educate, encourage, or enhance walking or bicycling to school. This information can be provided by the principal of the school or SRTS coordinator and include information pertaining to any:

- Walking/biking/safety curriculum taught by the school
- Frequency of and participation in encouragement programs
- Anything else that the school/district has done that makes walking and biking easier, safer, or the preferred transportation choice



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