

C/CAG

CITY/COUNTY ASSOCIATION OF GOVERNMENTS OF SAN MATEO COUNTY

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Millbrae • Pacifica • Portola Valley • Redwood City • San Bruno • San Carlos • San Mateo • San Mateo County • South San Francisco • Woodside*

2:30 PM, Thursday, June 15, 2017
San Mateo County Transit District Office¹
1250 San Carlos Avenue, 2nd Floor Auditorium
San Carlos, California

STORMWATER (NPDES) COMMITTEE AGENDA

- | | | |
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| 1. Public comment on items not on the Agenda (presentations limited to three minutes). | Breault | No materials |
| 2. Stormwater Issues from C/CAG Board meetings: <ul style="list-style-type: none">• June –Amendment Number 3 to the rain barrel rebate funding agreement with the Bay Area Water Supply and Conservation Agency, extending the term through June 30, 2018 for no additional cost (approved)• June – Authorizing the C/CAG Executive Director to execute Task Orders with EOA, LWA, and SGA in amounts not to exceed \$1,685,861, \$557,500, and \$325,000, respectively, for technical support services to the Countywide Water Pollution Program for Fiscal Year 2017-18 (approved) | Fabry | No materials |
| 3. ACTION – Review and approve April 20 Stormwater Committee meeting minutes | Fabry | Pages 1-7 |
| 4. INFORMATION – Announcements on stormwater issues <ul style="list-style-type: none">• PCBs in infrastructure – request for municipal partners• Other | Fabry | Verbal |
| 5. INFORMATION – Receive presentation on current status of member agencies in meeting trash load reduction requirements | Fabry/
Sommers | Page 8 |
| 6. INFORMATION – Receive presentation on current status of developing a Reasonable Assurance Analysis for implementing green infrastructure | Fabry/
Carter | Page 9 |
| 7. ACTION – Review and recommend approval of the draft call for projects for the Safe Routes to School and Green Streets Infrastructure Pilot Program | Bogert | Pages 10-33 |
| 9. Regional Board Report | Mumley | No Materials |
| 10. Executive Director’s Report | Wong | No Materials |
| 11. Member Reports | All | No Materials |

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C/CAG AGENDA REPORT

Date: June 15, 2017
To: Stormwater Committee
From: Matthew Fabry, Program Manager
Subject: Review and approve April 21, 2017 Stormwater Committee meeting minutes
(For further information or questions contact Matthew Fabry at 650 599-1419)

RECOMMENDATION

Review and approve April 21, 2017 Stormwater Committee meeting minutes, as drafted.

ATTACHMENTS

1. Draft April 21, 2017 Minutes

STORMWATER COMMITTEE
Regular Meeting
Thursday, April 20, 2017
2:30 p.m.

DRAFT Meeting Minutes

The Stormwater Committee met in the SamTrans Offices, 1250 San Carlos Avenue, San Carlos, CA, 2nd floor auditorium. Attendance at the meeting is shown on the attached roster. In addition to the Committee members, also in attendance were Sandy Wong (C/CAG Executive Director), Matt Fabry (C/CAG Program Manager), Reid Bogert (C/CAG Stormwater Program Specialist), Kristin Kerr (EOA, Inc.) Azalea Mitch (Menlo Park), Grant Ligon (City of San Mateo), Jennifer Lee (City of Burlingame), Breann Liebermann (San Mateo County Office of Sustainability), Steve Tyler (Town of Atherton), Richard Chiu (Daly City), and Keegan Black (City of Brisbane). Vice Chair Walter called the meeting to order at approximately 2:45 p.m.

1. Public comment: None

2. C/CAG staff Matt Fabry stated there were no stormwater related updates from the April annual retreat C/CAG Board meeting, which focused on transportation. Fabry introduced Denise Hutten, the newly designated committee member representing the City of Half Moon Bay, and Steve Tyler, who was attending on behalf of the Town of Atherton.

3. ACTION – The draft minutes from the March 16, 2017 Stormwater Committee meeting were unanimously approved as drafted (motion: Oskoui, second: Willis).

4. INFORMATION – Announcements on stormwater issues:

Stormwater funding - Fabry announced three open stormwater funding solicitations. Only the EPA grant has a 1-to-1 non-federal match requirement, and municipalities are highly encouraged to apply. All proposal deadlines are in May.

- California Natural Resources Agency Urban Greening Grant
- EPA Water Quality Improvement Fund (likely last funding for next four years)
- California Coastal Conservancy Prop 1 – most recent of quarterly calls for urban greening grants

Regional Roundtable - Fabry provided an update on the EPA-funded BASMAA project for a Regional Roundtable series addressing barriers to integrating transportation and stormwater projects. Announced the kick-off Roundtable meeting on March 28, 2017, which was well attended with around 100 participants from various agencies and organizations, including relevant funders such as the Natural Resources Agency, the Strategic Growth Council, the State Water Board, FEMA, MTC, and others. There will be additional Roundtable meetings of a more technical nature, with the final goal of creating a “road map” for overcoming barriers to integrating green infrastructure into transportation projects.

Reasonable Assurance Analysis update – Fabry updated the committee on the RAA, which must be submitted in the 2020 Annual Report as required in the MRP and must demonstrate compliance with the TMDLs for mercury and PCBs in the Bay through green infrastructure and additional control measures with milestones for 2020, 2030 and 2040. C/CAG staff, Chair Breault, Vice Chair Walter and Steve Carter from Paradigm Environmental presented the final slides on the revised calculations for PCBs and mercury loading from sediment transport in San Mateo County to Regional Water Board staff on March 28, 2017. Provided methodology on calibration of the hydrologic and sediment transport modeling and

demonstrated San Mateo's lower estimated sediment associated pollutant loading relative to the population based allocation in the TMDL. Water Board staff were concerned that this lower number is inconsistent with what's written into the TMDL, especially if other countywide programs develop similar results in which case the TMDL may be jeopardized, and also that these results are inconsistent with some monitoring data from local watersheds. Fabry explained the purpose of the BASMAA project on RAA guidance, which will establish accepted boundaries for various modeling approaches. This process has slowed down Paradigm's work for San Mateo County, but we expect resolution on the San Mateo approach within this fiscal year. Vice Chair Walter shared that Water Board staff Tom Mumley was very receptive to the presentation, and that Carter provided strong, defensible answers to his questions. Vice Chair Walter asked if the BASMAA RAA guidance document will change Paradigm's approach. Fabry stated the approach would not change, but San Mateo's findings demonstrate that the Water Board and other countywide programs may need to reconcile the fact that a population based loading allocation may not match the more accurately modeled numbers, and that Permittees will ultimately need to choose the path that best serves their interests within the somewhat fixed numbers in the TMDL, which are the waste load reduction requirements.

EPA Finance Forum – Fabry mentioned the EPA Stormwater Finance Forum held in Oakland on April 5, 2017. This was a follow-up to a similar event in Southern California. There were no major conclusions or answers to the problem of stormwater funding provided during the forum, but several good examples of how to better manage limited resources, especially with revenue generating constraints from Prop 218. Public/private partnerships were presented as a feasible approach to filling the stormwater funding gap, with a focus on performance based repayment to private owners and operators of green infrastructure, but this requires a dedicated revenue stream to pay back private entities. Fabry will send out links to presentations from the forum once they are posted.

Annual Report items – Fabry highlighted two new items pertaining to this year's Annual Reports:

- 1) Alternative to population based share of mercury/PCBs wasteload reduction – The MRP allows for Permittees to submit with their 2017 Annual Report an alternative approach to meeting the overall wasteload allocation for mercury/PCBs in the TMDL, via green infrastructure, other control measures and the adoption of a program to manage PCB-containing materials during building demolition. Because of the short timing for proposing an alternative written into the MRP, with respect to development of the RAA, Paradigm Environmental has not yet looked at scenarios to more cost-effectively achieve compliance for San Mateo County's overall wasteload reduction requirements. This work will be done in the first few months of next fiscal year. There are still questions about whether a countywide or jurisdictional approach is optimal, and Fabry has emailed Water Board staff to ask if Permittees can submit an alternative approach after the Annual Reports are due on September 30.
- 2) Exemption from building demolition program for managing PCBs-containing materials – Jurisdictions seeking exemption from the building demolition program must request exemption in their 2017 Annual Report. The MRP requires all Permittees to adopt a protocol for managing materials with PCBs with concentrations of 50 ppm or greater during demolition so that PCBs do not enter the MS4. Permittees can request exemption if evidence is provided to show that only structures that existed pre-1980 within its jurisdiction were single family residential and/or wood/frame buildings. Fabry suggested there will likely be no exemptions in San Mateo, but C/CAG will provide guidance to jurisdictions that may seek an exemption. Vice Chair Walter suggested this might be something the PCB workgroup might address as an inquiry to the group.

Unfunded Mandate Test Claims – Fabry shared background on the unfunded mandate test claims filed by all C/CAG member agencies except one jurisdiction under MRP 1.0 (2009/10). All agencies are filed under the City of Brisbane's claim, since Brisbane was the first to file. As an update, the Commission on

State Mandates (Commission) has been understaffed since the initial claims were filed, and there is an assumption that the Commission is also waiting for the results of proceedings for the test claims in Los Angeles and San Diego counties. The Supreme Court has already ruled in favor of LA County, but the case will go back to trial for further consideration of certain components of the test claims. C/CAG's contracted legal firm, Meyers Nave, informed Fabry that the Commission has issued a notice of "incompleteness" of the claims filed by San Mateo jurisdictions. Meyers Nave stated, however, that these issues are linked to new Commission staff assigned to the claims and should be resolvable. There may be a need for new cost estimates of the unfunded mandate test claims, which have not yet been budgeted for.

Committee members had several questions regarding these announcements. Committee member Donohue asked whether C/CAG had budgeted for an MRP reporting workshop this year, and if so, will there be a greater focus on GI requirements. Fabry confirmed and stated that much of the load reduction accounting work will be done by C/CAG, especially this year as this is the first year that Permittees will be reporting on GI for PCBs and mercury loads reduced. Fabry mentioned that EOA would provide Annual Report review and guidance for all member agencies, as in previous years.

Committee member Willis asked whether the building demolition program would necessarily require changes to local building codes. Fabry was uncertain about the extent of changes to local codes and ordinances, but said the BASMAA project to develop the program would inform this question. It was also asked whether the jurisdictions will be prepared enough to go to Council with GI Workplans this June. Fabry highlighted the various workplan components coming out of the GI TAC, including a model workplan, staff report, table of contents, etc., and asserted that staff should have all the resources needed at this point to craft a workplan for approval by Councils/City Managers. He also reasserted that the Water Board is more interested in having Permittees show through the approval of workplans, including a budgetary and staff resource commitment, to implementing the GI Plans, which are due with the 2019 Annual Reports, rather than the actual content of the workplans. Fabry also confirmed that he would be available for presenting to City Councils to help inform Councils of the GI planning process required by the MRP.

Committee member Oskoui asked about the MRP petition to the State Water Board prior to MRP 2.0. Fabry reminded the committee that San Mateo County signed onto the Santa Clara petition focusing on the issue of PCBs being a numeric effluent limit, rather than an action level. The State Board declared a motion to review the petition beyond the standard 270-day timeframe. The Santa Clara petition will likely be reviewed after the appeals to the Los Angeles permit are resolved. Fabry confirmed there is a budget line item for FY17/18 to support the petition process in the next fiscal year. Finally, in response to a question about the possibility of reissuing a Prop 218 process on behalf of the County, Fabry established there was not enough political support last time to move forward with a Prop 218 process (with an estimated \$37 million shortfall annually needed to adequately fund stormwater programming), but that there is a \$500,000 placeholder for FY17/18. There is no active contract with a consultant for supporting this process, should the C/CAG Board approve this as an action item next fiscal year.

5. ACTION – Fabry provided background on the issue of the C.4/C.5 compliance for business inspections and illicit discharge detection and elimination, as addressed by the Regional Water Board's Jan 30, 2017 letter to the 17 cities with active agreements with the San Mateo County Environmental Health Services Division (CEH) for stormwater inspections and Matt Fabry as the program manager for SMCWPPP. Fabry recapped that Heather Forshey, Director of CEH, spoke at the March Stormwater Committee meeting and informed committee members that due to cost-recovery concerns, CEH would be terminating all agreements with the cities by the end of the calendar year. The Stormwater Committee approved a response letter to the Regional Water Board, due March 30, 2017, asking for a one-month extension to provide a complete response, given the information received from CEH approximately a week before the original due date for the response letter. The extension was granted for submission no later than April 28, 2017.

The revised response letter acknowledges CEH's plans to dissolve the agreements by December 31, 2018 and includes the attached letter from Forshey informing the cities of this plan. The letter also acknowledges that CEH will continue services through the calendar year and includes the plan and efforts already made to ensure compliance with the MRP, specifically through updates to the cities' ERPs and BIPs. The letter specifies that all compliance issues will be addressed through these updates, rather than through updating the ERPs/BIPs and the Agreements with CEH, since these agreements will be terminated by the end of the calendar year. Where there are questions specifically about the County's role in business inspections the letter references CEH's ERP/BIP, which are in substantial compliance with the MRP, according to the Regional Water Board. The letter and the updated BIP/ERP templates also use agnostic language about contracting stormwater inspections, so that future contracting arrangements will easily fit into these documents. Fabry confirmed he will send the updated BIP/ERP templates to city staff for review prior to the April 25 CII workshop, where staff will have a chance to make jurisdiction-specific adjustments and additions to their templates.

Fabry also mentioned that he attended the City Managers meeting on April 13, and that it was concluded by Louise Rogers, County Health System Chief, that even with additional resources, the stormwater inspections program would need to be a stand-alone program with dedicated staff, and this is not an option for the County at this time. The County Manager also stated the inspection program would not be extended through 2017-18 fiscal year and would terminate at the end of 2017. Several options for the future of the business inspection program were discussed at the meeting, including San Mateo sharing more about their stormwater inspection program conducted in-house or Daly City, South San Francisco, and San Mateo conducting inspections through agreements with nearby jurisdictions (as agencies already performing all stormwater inspections in their jurisdictions). Fabry also mentioned the County made a spreadsheet of other countywide approaches to C.4 inspections which can be shared. The draft response letter was approved unanimously (motion: Oskoui, second: Ocampo).

6. INFORMATION – Fabry provided information on the preliminary C/CAG stormwater budget for FY17/18. Presented the initial estimates for revenue via the property tax and Measure M vehicle license fee and established a Revenue/Available Funds amount of \$2,536,000 and a starting balance of \$1,295,000 for FY17/18. With expected expenditures of roughly \$3,244,000, the anticipated ending balance would be \$587,000, with \$500,000 of that being reserved for a funding initiative. There is an additional reserve balance of \$120,000. The total available balance for FY18/19 would then be \$87,000. C/CAG will need to have internal discussions about how to best use 5% administrative cost allowance from the Measure M vehicle license fee. Looking ahead, the program is moving toward less dependency on rollover balance and more dependency on revenue only, which means there will likely need to be cuts to the budget now or in the future, especially regarding discretionary consulting costs. Fabry suggested that some larger budget items, especially water quality monitoring, will not decrease over time and will likely be best managed at the countywide level, but portions of that work are discretionary. For example, conducting ongoing monitoring to identify source properties with elevated PCBs concentrations is not mandated by the permit. These efforts do, however, support the Permittees in identifying potentially high priority sites and source properties for referral to the Water Board, which could secure significant PCB load reduction credits, where otherwise these reductions would need to be met through C.3 projects and green street or regional stormwater projects in the public right-of-way. Trash assessments are another high cost service currently being provided by EOA Inc., which the agencies could take on individually. There are areas of budget reductions in the coming years. Specifically, the Stormwater Resource Plan work is complete and the GI planning work will largely shift to the local level after this next fiscal year.

Fabry explained that C/CAG staff will be reviewing the proposed scopes of work from consultants in more detail and will look more carefully for potential cuts for FY17/18. The draft budget will be brought

to the C/CAG Board in May, and the final budget will be recommended for approval at the June C/CAG Board meeting.

Members discussed the question of a Prop 218 funding initiative, recognizing the need for more funding in future years, especially with increased mandates. It was asked if the next opportunity to propose a funding initiative would be 2018. Fabry clarified that for property related fees, there's no restriction on timing for mailing out the ballot initiative and a simple majority of property owners is required for approval. The committee also discussed SB 231 proposed by Senator Robert Hertzberg, which would clarify the term "sewer" in the Prop 218 Omnibus Implementation Act to include storm sewers. If successful, then property related sewer fees in San Mateo County could be implemented without voter or property owner approval, but still would be subject to the majority protest process, similar to proposed water and sewer rate increases. Fabry mentioned that even if the bill passes and is signed into law, it would likely be challenged in court and the only certain option would be to have a constitutional amendment. It was also mentioned that the potential 2018 regional toll measure and a Transportation Authority measure may preclude the political feasibility of a Prop 218 initiative. It was recommended by the committee that C/CAG plan ahead and perhaps work with the Ad-hoc Committee to develop a strategy for a strong pitch to the C/CAG Board if a funding initiative is agreed to. In general, the Committee agreed that the budget was in good standing and should be supported, because SMCWPPP provides invaluable services to the member agencies.

Fabry concluded by saying that with the upcoming RAA results and modeling scenarios for GI implementation, there will be better opportunities to discuss funding approaches, which will include conversations about how exactly funds will be spent, e.g., distribution of green streets vs. regional capture projects and actual plans for implementation. This effort will also provide a more presentable story to the public of how stormwater funding from a Prop 218 initiative would be used. Fabry said the reserve funds for the funding initiative would be maintained in the budget, based on this conversation.

7. Regional Water Board Report: NONE.

8. Executive Director's Report: C/CAG Executive Director, Sandy Wong, shared that the State Legislature passed SB 1 in both houses, and that due to Matt Fabry's efforts in coordination with C/CAG's lobbyist was able to include stormwater language for roads and highway projects. Executive Director Wong also shared that the C/CAG Water Committee is scheduled for its kick-off meeting on May 17 and that the agenda will be distributed soon.

11. Member Reports: NONE.

Vice Chair Walter adjourned the meeting at 3:55 p.m.

2017 Stormwater Committee Roster			Jan	Feb	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec
Agency	Representative	Position												
Atherton	Marty Hanneman	City Engineer				O								
Belmont	Afshin Oskoui	Public Works Director	X		X	X								
Brisbane	Randy Breault	Public Works Director/City Engineer	X		X	O								
Burlingame	Syed Murtuza	Public Works Director	X		X	O								
Colma	Brad Donohue	Director of Public Works and Planning	X	C		X	C							
Daly City	John Fuller	Public Works Director	O	A	X	X	A							
East Palo Alto	Kamal Fallaha	City Engineer		N	X		N							
Foster City	Jeff Moneda	Public Works Director	X	C	X	X	C							
Half Moon Bay	Denice Hutten	Associate Engineer		E		X	E							
Hillsborough	Paul Willis	Public Works Director	X	L	X	X	L							
Menlo Park	Justin Murphy	Public Works Director	X	E	X	X	E							
Millbrae	Ray Chan	Public Works Director		D			D							
Pacifica	Van Ocampo	Public Works Director/City Engineer	X			X								
Portola Valley	Howard Young	Public Works Director			X									
Redwood City	Saber Sarwary	Supervising Civil Engineer	X		X									
San Bruno	Jimmy Tan	City Engineer			X	X								
San Carlos	Jay Walter	Public Works Director	X		X	X								
San Mateo	Brad Underwood	Public Works Director				X								
South San Francisco	Eunejune Kim	Public Works Director	X			X								
Woodside	Sean Rose	Public Works Director	X		X									
San Mateo County	Jim Porter	Public Works Director	X		X	X								
Regional Water Quality Control Board	Tom Mumley	Assistant Executive Officer	O											

"X" - Committee Member Attended
 "O" - Other Jurisdictional Representative Attended

C/CAG AGENDA REPORT

Date: June 15, 2017
To: Stormwater Committee
From: Matthew Fabry, Program Manager
Subject: Receive presentation on current status of member agencies in meeting trash load reduction requirements

(For further information or questions contact Matthew Fabry at 650 599-1419)

RECOMMENDATION

Receive presentation on current status of member agencies in meeting trash load reduction requirements.

BACKGROUND

The Municipal Regional Permit (MRP) requires specific reductions in trash discharges from 2009 levels. Permittees are required to achieve 70% reduction by July 1, 2017 and 80% reduction by July 1, 2019, with the goal of 100% reduction or no adverse impact to receiving waters from trash by July 1, 2022.

C/CAG's consultant, EOA, has been assisting C/CAG member agencies with the trash load reduction requirements since 2009. EOA staff will present a status report on the progress that C/CAG member agencies have made to-date on reducing trash loads during FY 16-17. Trash reduction estimates will be summarized and compared to mandated reductions required in the MRP. EOA will also present next steps for C/CAG member agencies in preparation for the September submittal of annual reports, along with recommendations for agencies that may not achieve the MRP 70% trash reduction requirement.

ATTACHMENTS

None

C/CAG AGENDA REPORT

Date: June 15, 2017
To: Stormwater Committee
From: Matthew Fabry, Program Manager
Subject: Receive presentation on current status of developing a Reasonable Assurance Analysis for implementing green infrastructure

(For further information or questions contact Matthew Fabry at 650 599-1419)

RECOMMENDATION

Receive presentation on current status of developing a Reasonable Assurance Analysis for implementing green infrastructure.

BACKGROUND

The Municipal Regional Permit (MRP) requires specific reductions in mercury and PCBs (polychlorinated biphenyls) by the end of the permit term (2020) and at an interim point (June 30, 2018), and further specifies a portion of the required load reductions be achieved via green infrastructure. The MRP requires a Reasonable Assurance Analysis (RAA) to demonstrate load reductions required via green infrastructure will be achieved by 2040. Paradigm Environmental has been developing the RAA for C/CAG's member agencies.

Paradigm Environmental will provide an overview of the status of the RAA, including a discussion of the modeling and cost-optimization of green infrastructure and regional projects. The presentation will include examples of preliminary modeling output to provide an understanding of the modeling system and metrics that can be used to support green infrastructure planning to meet pollutant load reduction allocations.

ATTACHMENTS

None

C/CAG AGENDA REPORT

Date: June 15, 2017

To: C/CAG Stormwater Committee

From: Reid Bogert

Subject: Review and recommend approval of draft call for projects for the Safe Routes to School and Green Streets Infrastructure Pilot Program

(For further information or questions, contact Reid Bogert at 650-599-1433)

RECOMMENDATION

Review and recommend approval of draft call for projects for the Safe Routes to School and Green Streets Infrastructure Pilot Program

FISCAL IMPACT

Up to \$2 million.

SOURCE OF FUNDS

Funded in equal parts by local \$4 vehicle license fees (AB 1546) designated for regional stormwater pollution prevention programs and \$10 vehicle license fees (Measure M) designated for Safe Routes to School programs.

BACKGROUND

C/CAG staff is developing a call for projects for the Safe Routes to School (SRTS) and Green Streets Infrastructure Pilot Program (Pilot Program), which is intended to fund integrated improvements within the public right-of-way that increase safety for children walking and biking to school, while also improving water quality, increasing urban greening, and enhancing the pedestrian environment. A primary goal of the Pilot Program is to demonstrate that green infrastructure and pedestrian improvements can be cost-effectively integrated to increase safety and achieve stormwater goals for C/CAG's member agencies. The Pilot Program is intended to fund combinations of vegetated curb extensions and pedestrian bulbouts/curb ramps with crossings/stripping at intersections or mid-block crossings near schools.

C/CAG will provide up to \$2 million in equal shares of local vehicle license fees designated for SRTS and stormwater pollution prevention to fund the Pilot Program. The Pilot Program is designed to provide grant funds with equal shares of SRTS and stormwater funds; as such, proposed projects must include balanced combinations of SRTS and stormwater features. Funds are available to C/CAG member agencies, and each agency may apply for a minimum of \$100,000 and a maximum of \$250,000 per project, with a two project limit per jurisdiction. If applying for funding for two projects, the proposed individual projects should be geographically separate or otherwise functionally distinct. Funds are for construction projects and costs only (planning, design, or staff time is not eligible for funding). There is a 15% local cash match requirement to further leverage C/CAG's funding.

Project locations must either be identified in an existing Walk Audit or Comprehensive Bicycle and Pedestrian Plan, and should directly benefit children walking or biking to/from a nearby public or private school. If not identified in a local Walk Audit or Bicycle/Pedestrian Plan, projects should occur within ½ mile of a school and provide reasonable justification for eligibility (e.g., accident statistics or other supporting data). The Pilot Program will not fund non-infrastructure projects, or ineligible project elements, including pedestrian or street lighting, illuminated crosswalks, or rectangular rapid flashing beacons (RRFB), which would potentially create an unbalanced cost distribution between stormwater and pedestrian features.

C/CAG and San Mateo County Office of Education staff co-hosted a coordination meeting on May 18, at which municipal representatives were paired with SRTS coordinators to learn about the planned solicitation and discuss potential project opportunities. C/CAG staff also plans to hold a pre-application workshop during the solicitation period to address specific questions from interested parties.

SRTS/Green Streets Infrastructure Project Schedule (tentative):

Event	Date
Call for Projects Issued	Mid-July, 2017
Applications Due	Friday, September 15, 2017
Selection Panel Reviews Applications	End of September, 2017
C/CAG Committees Review Selection Panel Recommendations	Thursday, October 19, 2017
C/CAG Board Considers Recommendations	Thursday, November 9, 2017
Execute Funding Agreements with Project Sponsors for Awarded Projects	Thursday, March 01, 2018
Construction Complete	Monday, October 01, 2019
Final Reimbursement Requests Due	Monday, December 31, 2019

ATTACHMENTS

1. Draft Safe Routes to School and Green Streets Infrastructure Call for Projects
2. Draft Safe Routes to School and Green Streets Infrastructure Application

ATTACHMENT 1 - Draft Safe Routes to School and Green Streets Infrastructure Call for Projects



*Atherton • Belmont • Brisbane • Burlingame • Colma • Daly City • East Palo Alto • Foster City • Half Moon Bay • Hillsborough • Menlo Park
Millbrae • Pacifica • Portola Valley • Redwood City • San Bruno • San Carlos • San Mateo • San Mateo County • South San Francisco • Woodside*

Safe Routes to School (SRTS) and Green Streets Infrastructure Pilot Program Funding Guidelines

Background

The City/County Association of Governments of San Mateo County (“C/CAG”) is a joint powers agency whose members are the County and the 20 cities and towns in San Mateo County. Its primary role is a Congestion Management Agency, but it has also administered the Countywide Water Pollution Prevention Program since its inception in the early 1990s, with a primary goal of assisting member agencies in meeting municipal stormwater regulatory mandates.

The San Mateo County Safe Routes to School Program (“SRTS”) is a partnership between C/CAG and the San Mateo County Office of Education (“SMCOE”), the goal of which is to encourage and enable school children to walk and bicycle safely to school and reduce congestion and emissions caused by school related travel. The program primarily focuses on non-infrastructure projects and activities to improve health and safety and reduce traffic congestion.

Project Call

The Safe Routes to School and Green Streets Infrastructure Pilot Program (“Pilot Program”) is intended to fund integrated improvements within the public right-of-way that increase safety for children walking and biking to school, while also improving water quality, increasing urban greening, and enhancing the pedestrian environment. The focus of the Pilot Program is integrated improvements at intersections or mid-block crossings, all within the public right-of-way.

A primary goal of the Pilot Program is to demonstrate that green infrastructure can be cost-effectively integrated with traditional Safe Routes to School infrastructure projects to enhance safety and to achieve stormwater pollutant load reductions for C/CAG’s member agencies, in accordance with the Municipal Regional Stormwater Permit (MRP). C/CAG will provide up to \$2.0 million to fund the Pilot Program through combining equal amounts of funding designated for SRTS and stormwater management. Funding is provided from local vehicle license fees designated for congestion management and pollution prevention. Applicants are eligible to apply for multiple project locations, but awards will be limited to two project locations per applicant, with a maximum grant award of \$250,000 per project location.

Eligible Applicants

Only local governments (cities, towns, and the County) in San Mateo County are eligible applicants for funding through the program. Although a local agency may choose to collaborate with a school district to design, build, or maintain a proposed project, the applying agency will be responsible for project delivery and ensuring sustained implementation of an operations and maintenance plan.

Eligible Project Locations

Proposed project locations should have been previously mentioned in a Walk Audit or Comprehensive Bicycle and Pedestrian Plan, and should directly benefit children walking or biking to a public or private school. If not mentioned in a previous Walk Audit or Pedestrian Bike Plan, the proposed project must be within a ½ mile radius of a school and other reasonable justification should be provided, such as accident statistics or other data as to why the particular location is deemed appropriate.

The pilot program seeks to fund projects entirely in the public right-of-way; however, proposed projects that occur primarily in the right-of-way, but have minimal connection with school property (e.g., a sidewalk connecting to a curb ramp, bulb out or vegetated curb extension) and still adhere to the goals of integrating SRTS and green infrastructure, may be considered on a case by case basis.

Individual project locations, eligible for up to \$250,000 each with a limit of two projects per applying jurisdiction, should be geographically separate or otherwise functionally distinct so as to be designated as individual projects. If applying for funding for two project locations (such as two intersections in close proximity), the local agency should provide sufficient information regarding the designation as separate projects.

Eligible Activities

This pilot program will fund infrastructure projects only (i.e., planning, outreach, and other non-infrastructure projects are ineligible for funding), and only construction capital costs are eligible for funding. Any staff time and overhead costs are not eligible for reimbursement. See **Funding Details** below for information on match requirements and eligible costs.

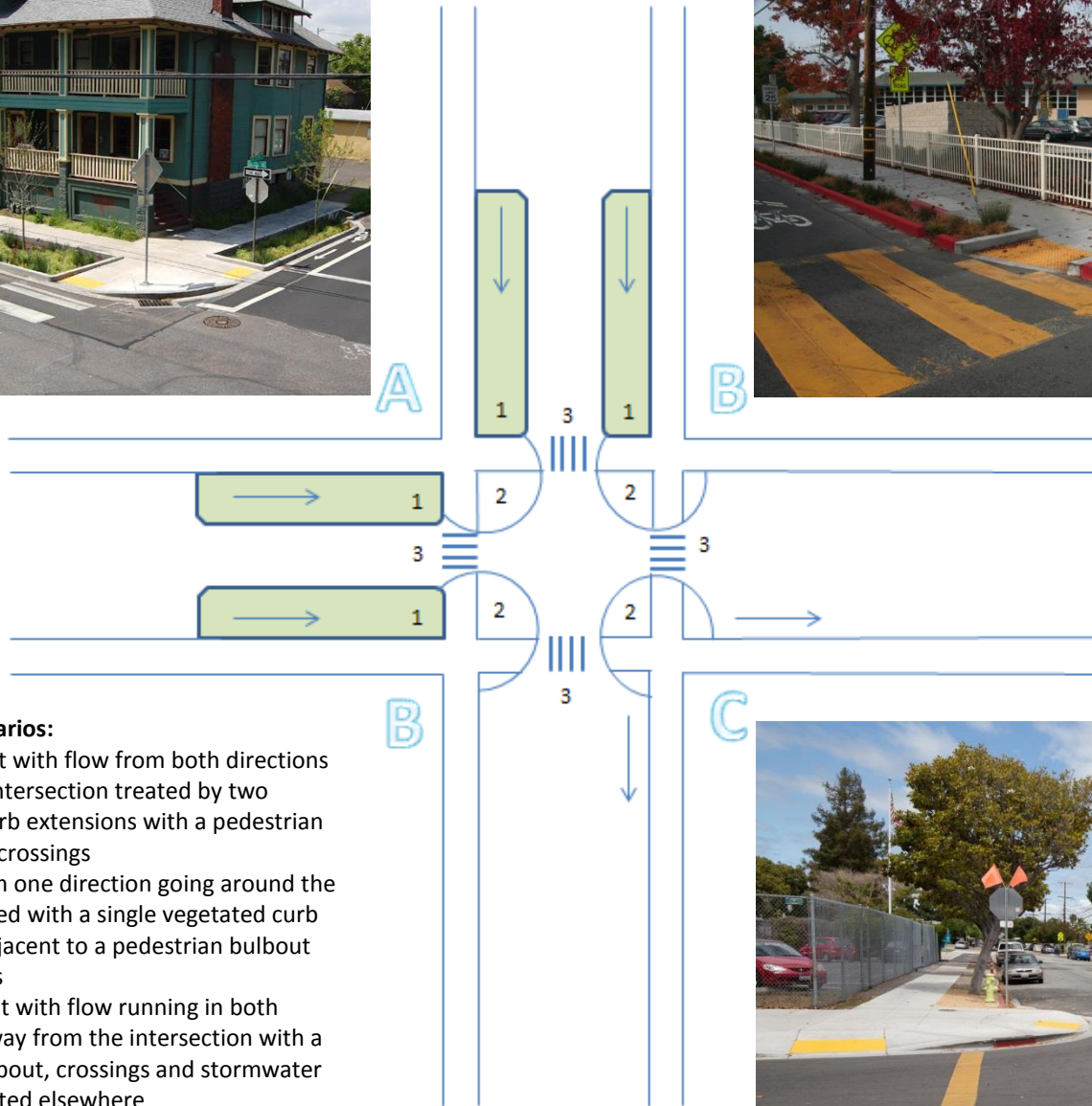
Eligible Project Elements

The intention of the pilot program is to fund integrated stormwater management and SRTS projects in the right of way. Therefore, eligible project elements should include a balanced combination of vegetated curb extensions with pedestrian enhancements at intersections or mid-block crossings. C/CAG staff has created a series of potential scenarios occurring at a model intersection and mid-block crossing, shown in Figures 1 and 2 below. These scenarios demonstrate the intention of this pilot program and should help guide development of project proposals.

Eligible project elements could include:

- Vegetated curb extensions (1)
- Pedestrian bulb outs/curb ramps (2)
- Pedestrian striping/crosswalks (3)

Scenarios



Project Scenarios:

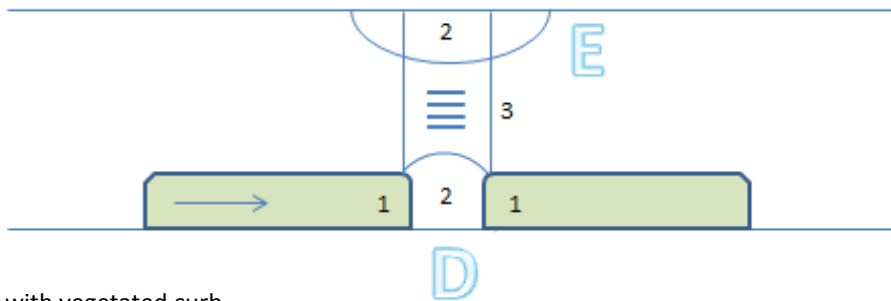
A – Low point with flow from both directions toward the intersection treated by two vegetated curb extensions with a pedestrian bulbout and crossings

B – Flow from one direction going around the corner, treated with a single vegetated curb extension adjacent to a pedestrian bulbout and crossings

C – High point with flow running in both directions away from the intersection with a standard bulbout, crossings and stormwater features located elsewhere



Figure 1. Diagram of eligible project elements at an example four-way intersection



Project Scenarios:

D – Mid-block crossing with vegetated curb extensions on either or both side(s) of crossing

E – Mid-block crossing with a standard bulbout and stormwater features located elsewhere

Figure 2. Diagram of eligible project elements at an example mid-block crossing

Project elements can include any individual or combination of scenario(s) A-E in Figures 1 and 2. These are generalized conditions and are meant to guide applicants toward identifying eligible project components. Site conditions and intersection retrofits will vary; however, **all proposed projects should demonstrate an equal balance among stormwater and SRTS improvements** using the eligible project elements (1, 2, 3). For example, if an intersection improvement project only includes standard pedestrian bulbouts (e.g., no vegetated curb extensions to manage stormwater, as in scenarios C and E), due to specific site conditions and direction of stormwater flow, an additional scenario would need to be included that manages stormwater (e.g., scenarios A, B, or D) and C or D and C). Projects need not be constructed at four-way intersections. The pictures associated with each scenario are examples of what would be considered eligible project designs, and more example projects throughout San Mateo County are provided in the **Resources** section below.

In regard to sizing vegetated curb extensions, C/CAG is seeking to fund projects that will help in reducing pollutant loads, as required under the Municipal Regional Permit. As such, project proposals should include sizing details for the green infrastructure features. Project applicants should indicate whether the proposed green infrastructure elements meet the 4% “rule of thumb” sizing (treatment area to tributary drainage area), or have been sized more efficiently in accordance with the Countywide Water Pollution Prevention Program’s C.3 Technical Guidance manual. Proposals should delineate tributary drainage areas, stormwater flow direction, and locations of existing storm drain inlets in the project vicinity.

Project proposals should generally show a balance between SRTS and green infrastructure features and associated costs. In order to maintain relative balance between SRTS and stormwater costs within projects, the following pedestrian and green infrastructure enhancements will not be funded. Project proposals may include these elements, but the associated costs of these elements will not be reimbursed, and project applicants will need to identify these costs separately in the proposed budget.

This pilot program will NOT fund the following pedestrian enhancements:

- Pedestrian or Street Lighting
- Rectangular Rapid Flash Beacon (RRFB)
- Illuminated Crosswalks
- Raised Crosswalks

This pilot program will NOT fund the following stormwater features:

- Landscaping elements that are not designed to capture and manage stormwater, unless proposed as part of a pedestrian bulb out that is not suitable for stormwater management due to flow direction or other constraints
- Porous pavement/asphalt/concrete in-lieu of vegetated curb extensions/bulb outs

Funding Details

There is a total of up to \$2.0 million dollars available under the current call for projects. Grant funds may only be used to fund construction costs. A minimum of \$100,000 and a maximum of \$250,000 will be awarded per project (two project limit per jurisdiction). A local cash match of 15 percent of construction costs is required for program eligibility.

This integrated pilot program is a cost reimbursement program, and all reimbursements will be made after documentation of the completed project is submitted with invoices. Part of the purpose of the pilot program is to determine the relative cost share between SRTS and stormwater elements of integrated projects. Reimbursement requests (including photo documentation of completed projects) should detail final project construction costs, and provide best estimates of the share of costs split between the two programmatic elements. Indirect costs, including any staff time, will not be reimbursed.

The 15 percent local match will be applied to the physical construction costs, and this amount will be taken off of the top of construction costs when reimbursements are requested. Therefore, reimbursements will be 85 percent of physical construction costs, or the full amount of the grant

request, whichever is less. For example, a project that requests \$250,000 through the program, but which has \$275,000 in actual construction costs would be reimbursed \$233,750 at the end of the project. The reimbursed amount is equal to the actual construction cost less the 15 percent local match (\$41,250). If the actual construction costs were at least \$287,500 (which is the \$250,000 maximum per project amount, plus the 15 percent local match), then the agency would be reimbursed the full \$250,000 requested.

All projects must finish construction by October 1, 2019, and final invoices for reimbursement must be submitted by March 1, 2019. Extensions to these deadlines will be considered on a case by case basis.

Mandatory Application Elements

All submitted project applications must include a complete application form and the following as attachments or included in the application, as appropriate:

1. A letter of support from the participating school district, explaining how the proposed project will leverage existing SRTS programming or other efforts related to improve walking and biking conditions for children to and from school.
2. A map of the project area showing the project location, the location of the benefitting school(s), and any relevant land use or transportation information. Also include a walking or biking route map to the school, if applicable. Include documentation that proposed location is included in a walk audit or is within a ½ mile of a school with other relevant justification for the appropriateness of the location.
3. A schematic or conceptual design of the proposed SRTS and stormwater infrastructure elements, including the direction of stormwater flow and any relevant street characteristics, including storm drain infrastructure in the project vicinity. The schematic should also include delineations of drainage areas to and treatment capacities for each stormwater feature. Projects will be awarded full points for achieving the Municipal Regional Permit's Provision C.3.d. sizing criteria for the entire drainage area (including estimates for run-on from adjacent parcels) treated by the proposed stormwater features. At a minimum, the proposed features should be sized to treat the drainage area of the street (crown to curb) draining to the feature, using the 4% "rule of thumb" (treatment area to drainage area). More detailed sizing calculations are encouraged using the [SMCWPPP Provision C.3.d. sizing calculation sheet for combination flow and volume based criteria](#), however, as these will better help the selection committee in reviewing proposed projects and will ensure the facilities are appropriately sized.
4. A long-term operations and maintenance (O&M) plan for the stormwater features. The plan should include details (frequency and actions) about specific maintenance activities, including roles and responsibilities, and dedicated funding for the following operations and maintenance components:
 - a. Removal of trash/debris
 - b. Vegetation maintenance
 - c. Erosion control/mulch replacement
 - d. Aesthetics/safety

- e. Upkeep of overall function of the stormwater features

If there is an agreement between the applying local agency and the benefitting school as to who is responsible for O&M, this should be clearly described in the plan. It should also be recognized that ultimately the applying agency is responsible for ensuring proper long-term maintenance of the stormwater features.

*See [C.3 Technical Guidance Appendix G](#) for recommended O&M actions and an O&M template to maintain green streets stormwater features.

5. A scope of work, project budget, and schedule with specific timelines and tasks for expected deliverables, from design through construction. To the extent feasible, the proposed budget should show the expected cost distribution between the SRTS and green infrastructure components. The actual cost distribution will be reviewed during the reimbursement process, so all final bids and change orders must be submitted with reimbursement requests. The project budget must specify the requested grant amount, which should be calculated as the total estimated construction cost less the 15 percent local match to be applied toward construction. A 10 percent construction contingency cost line item is permitted. See example under **Funding Details**.

Recommended Project Elements

Project proposals will be awarded more points for including the following as attachments or within the application:

1. Community letters of support.
2. Integration of educational signage in the project design.
3. Projects benefitting schools that are participating in the San Mateo County Safe Routes to School (SRTS) Program. If the school is not participating in SRTS, schools may provide evidence of practicing SRTS initiatives (e.g., established in existing plans, participating in community outreach efforts to educate about walking and biking to school).
4. Projects that address localized drainage or flooding issues.
5. Projects that are identified in other local or countywide plans, or that directly support goals, objectives, or projects in other plans, including bike and pedestrian master plans, community-based transportation plans, complete or sustainable streets plans, etc. Applications for projects that provide such benefits to existing plans should identify the relevant plans and describe how the proposed project benefits or is identified in such a plan.
6. Readiness to Proceed – projects that are closer to construction-ready, will be awarded more points in the scoring process.

Resources

Project Examples – The following Google Street View links show infrastructure projects in San Mateo County that demonstrate integrated stormwater and pedestrian improvements at intersections that would qualify for funding by this pilot program (note: some projects show project elements that are ineligible for grant funding, such as flashing beacons and pedestrian-scale lighting).

[Burlingame Ave. and Park Rd., Burlingame](#)

[1651 Hillside Blvd., Colma](#)

[Delaware St. and E 16th St., San Mateo](#)

[Humboldt St. and College Ave., San Mateo](#)

[Mid-block crossing N Humboldt St., San Mateo](#)

[Laurel Elementary School, San Mateo](#)

[Arroyo Ave and El Camino Real, San Carlos](#)

[Old County Rd. and Riverton Dr., San Carlos](#)

Design Guidance – Below is a list of resources for guidance and typical designs, standards and details, as well as operations and maintenance considerations, for green streets stormwater infrastructure.

[C.3 Stormwater Technical Guidance](#)

[San Mateo County Sustainable Green Streets and Parking Lots Design Guidebook](#)

[Bay Area Urban Greening Intersection Retrofit Typical Details \(Report and Conceptual Designs under “Design Charrette” tab at bottom of page\)](#)

[SFPUC Stormwater Design Guidelines, Typical Details and Specifications](#)

[EPA – Elements of a Green Infrastructure Maintenance Business Plan](#)

Evaluation Criteria

Evaluation Criteria	Description	Max Points
Existing Conditions	The project addresses site-specific SRTS and stormwater management needs and demonstrates the benefits of integrating transportation/pedestrian road improvements with green infrastructure for stormwater management.	23
Proposed Project	Project has a well-defined scope of work and timeline identifying the key purpose and objectives.	37
Project Timeline and Budget	Timeline and budget for all phases of project, including information on match requirement (how much is provided and for what construction costs).	10
School and Community Support	Project demonstrates meaningful community support from the benefitting school district, school(s) and other community stakeholders.	30
Total		100

Application Submission

Applicants must submit 5 bound copies and 1 electronic copy of the completed application along with all of the required and supporting documents. All applications must be received at the C/CAG office by **Friday, September 15, 2017 at 5:00 pm**. A workshop for prospective applicants will be held on **XXX**.

Please submit applications to:

Reid Bogert, Stormwater Program Specialist
 C/CAG
 555 County Center, 5th Floor
 Redwood City, CA 94063

C/CAG Safe Routes to School and Green Streets Infrastructure Pilot Schedule (tentative)

Event	Date
Call for Projects Issued	Mid-July, 2017
Applications Due	Friday, September 15, 2017
Selection Panel Reviews Applications	End of September, 2017
C/CAG Committees Review Selection Panel Recommendations	Thursday, October 19, 2017
C/CAG Board Considers Recommendations	Thursday, November 9, 2017
Execute Funding Agreements with Project Sponsors for Awarded Projects	Thursday, March 01, 2018
Construction Complete	Monday, October 01, 2019
Final Reimbursement Requests Due	Monday, December 31, 2019

For any questions regarding the program or application process please contact Reid Bogert at 650-599-1433 or rbogert@smcgov.org.

Scorer:		Project Sponsor:		
Date Completed:		Project Title:		
Evaluation Criteria	Description	Instructions/Scale	Points Assigned	Additional Comments from Scorer
Section II. Existing Conditions	The project addresses site-specific SR2S and stormwater management needs and demonstrates the benefits of integrating transportation/pedestrian road improvements with green infrastructure for stormwater management.	<i>Add points from category A</i>	23	
A. Project Need	1. Project area map showing project location, benefitting school(s) and other relevant land use or transportation information. Walking or biking route map may also be included.	Mandatory No project area map - ineligible	NA	
	2. Project identifies current risks and/or obstacles (physical or perceived) to walking and/or bicycling to and from your school site(s), including for children with disabilities.	<i>Enter a number between 0 and 10</i> No need - 0 Low need - 5 High need - 10	10	
	3. Project identifies localized drainage or flooding issues and/or other community benefits that can be addressed through green infrastructure designs.	<i>Enter a number between 0 and 15</i> No need - 0 Low need - 3 High need - 5	5	
	4. Proposed project locations previously mentioned in a Walk Audit or Ped/Bike Plan, or the project is within 1/2 mile of school AND the sponsor provides reasonable justification.	<i>Enter a number between 0 and 5</i> Poor justification - 0 Satisfactory justification - 3 Strong justification OR project has been mentioned in Walk Audit or Ped/Bike Plan - 5	5	
	5. Project provides direct benefits to an existing local or countywide plan, or directly supports goals, objectives, or projects in other plans, including bike and pedestrian master plans, community-based transportation plans, complete or sustainable streets plans, etc. Evidence is provided identifying the connection between the proposed project and such plans.	<i>Enter a number between 0 and 3</i> Does not provide benefits to an existing plan - 0 May provide some benefit to an existing plan, but lack of evidence - 1 Demonstrates direct benefit to an existing plan and includes strong supporting evidence - 3	3	
Section III. Proposed Project	Project has a well-defined scope of work and timeline identifying the key purpose and objectives.	<i>Add the number of points from categories A-C</i>	37	
A. Project Components	1. Project helps address the problems identified in the Existing Conditions and clearly explains how the infrastructure components will address SRTS and stormwater goals through a balanced and integrated approach, referencing the schematic diagram (scenarios A-D) in the application or providing an alternative schematic and explanation if needed.	<i>Enter a number between 0 and 10</i> Incomplete description/schematic of scenario identified for integrating SRTS and stormwater components - 0 Satisfactory description/schematic of scenario identified for integrating SRTS and stormwater components - 5 Strong scope of work with clearly explained schematic of scenarios - exceeds basic required components in application - 10	10	
	2. Project explains clearly how SRTS and stormwater planning and infrastructure will be integrated. Proposal includes a balanced approach to integration, both in terms of cost distribution and the allocation of project features.	<i>Enter a number between 0 and 3</i> Poorly balanced project components - 0 Adequately balanced project components demonstrated through schematic scenarios or alternative descriptions - 2 Very well balanced project components demonstrated through schematic scenarios or alternative descriptions - 3	3	
	3. Simple design concept of proposed project components, including calculations of treatment capacity for stormwater features and relevant maps delineating drainage areas.	Mandatory - <i>Enter a number between 0 and 10</i> No design concept - ineligible Poorly developed design concept (lack of detail, missing information) - 2 Satisfactory design concept - 5 Strong design concept (includes excellent detail and planning, along with all calculations for stormwater treatment and associated map delineating drainage areas) - 10	10	

	4. Project includes educational signage to raise awareness about the purpose and value of green infrastructure/SRTS infrastructure	<p><i>Enter a number between 0 and 2</i></p> <p>No educational signage - 0</p> <p>Educational signage included in scope - 2</p>	2	
<i>B. Logistics</i>	1. Does the proposed project/project sponsor have ROW clearance for all property involved?	<p><i>Mandatory (yes/no)</i></p> <p>No clearance - ineligible</p>	NA	
<i>C. Operations & Maintenance</i>	1. Proposal identifies a plan for future long-term operations and maintenance of the stormwater features, including a description of necessary maintenance activities, frequency of activities, who will be responsible for O&M, as well as the plan for dedicated funding.	<p><i>Mandatory - Enter a number between 0 and 10</i></p> <p>No plan for funding O&M - ineligible</p> <p>Plan for O&M, but no dedicated funding - 2</p> <p>Satisfactory funding plan for O&M with description of activities, frequency and responsibilities - 5</p> <p>Strong O&M funding plan above and beyond expectations to ensure long-term maintenance - 10</p>	10	
<i>D. Project Readiness</i>	1. Project demonstrates readiness to proceed with detailed project concept, budget, timeline.	<p><i>Enter a number between 0 and 2</i></p> <p>Low readiness to proceed - 0</p> <p>Medium readiness to proceed - 1</p> <p>High readiness to proceed - 2</p>	2	
Section IV. Project Timeline and Budget	<i>Timeline and budget for all phases of project, including 15% match requirement contributed to total construction costs. Budgets should also specify cost distributions for SRTS and GI features to extent possible.</i>	<i>Total possible points - 10</i>	10	
<i>A. Timeline and Budget</i>	1. The proposal includes a reasonable project budget and timeline with key dates for all phases of the project, including planning, design and construction. Budget should include 15% match from project sponsor, and may include a 10% contingency applied to construction costs. The proposed budget should also provide a cost distribution for SRTS and GI features and specify the requested grant amount, accounting for the 15% match requirement applied to construction costs.	<p><i>Mandatory - Enter a number between 0 and 10</i></p> <p>No budget and timeline - ineligible</p> <p>Weak budget and timeline with missing components - 3</p> <p>Satisfactory budget and timeline without missing components - 5</p> <p>Strong budget and timeline - complete, clearly defined and well-organized - 10</p>	10	
Section V. School and Community Support	<i>Project demonstrates meaningful community support from the benefitting school district, school(s) and other community stakeholders.</i>	<i>Add points from category A</i>	30	
<i>A. School and Community Support</i>	1. In applying for the SRTS/GI application, the highest authorized representative of the applying school district must submit a letter of support	<p><i>Mandatory - Enter a number between 0 and 15</i></p> <p>No support - ineligible</p> <p>Weak support - 3</p> <p>Moderate support - 7</p> <p>Strong support - 15</p>	15	
	2. Application includes additional letters of support (LOS) from the community affected by the project.	<p><i>Enter a number between 0 and 10</i></p> <p>No additional LOS - 0</p> <p>Additional letter(s) - medium quality/need - 5</p> <p>Additional letter(s) - high quality/need - 10</p>	10	
	3. The school either participates in the SRTS Program or can show evidence of SRTS initiatives.	<p><i>Optional - Enter a number from 0-5</i></p> <p>Not Participating in SRTS/No Evidence of SRTS Initiatives - 0 points</p> <p>Not Participating In SRTS/Some Evidence of SRTS Initiatives - 2/3 points</p> <p>Participating in SRTS/Substantial Evidence of SRTS Initiatives - 5 points</p>	5	
			100	

ATTACHMENT 2 - Draft Safe Routes to School and Green Streets Infrastructure Application



Safe Routes to School (SRTS) and Green Streets Infrastructure Pilot Program Project Application

Section I: General Project and Applicant Information

General Project Information

Sponsor Agency:

Project Title:

Amount of Funds Requested (\$):

Note: Minimum request is \$100,000 and maximum award is \$250,000 per project location (2 project limit per applying jurisdiction)

Participating School District:

Participating School Name(s) & Address(es):

Contact information of District Safe Routes to School Coordinator:

Project Manager

Name:

Title:

Agency:

Phone Number:

E-mail Address:

Mailing Address:

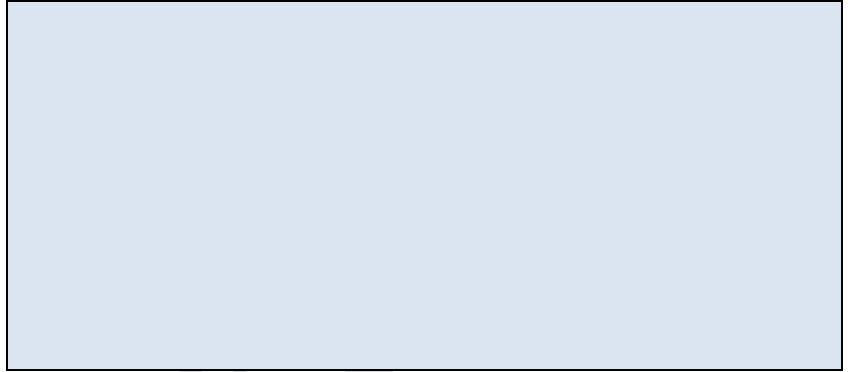
City, State, Zip:

Section II: Existing Conditions

A. Project Need

1. Description of project location and boundaries:

Please attach a map of the project area showing the project location, the location of the benefitting school(s), and any relevant land use or transportation information. Also include a walking or biking route map to the school if applicable.

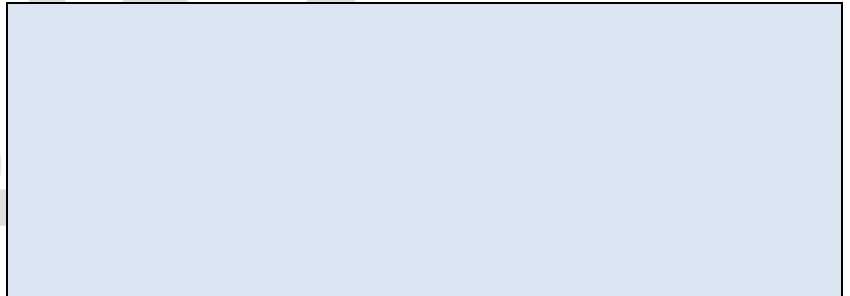


2. Why is the project needed?

What are the current risks and/or obstacles (physical or perceived) to walking and/or bicycling to and from your school site(s), including for children with disabilities?



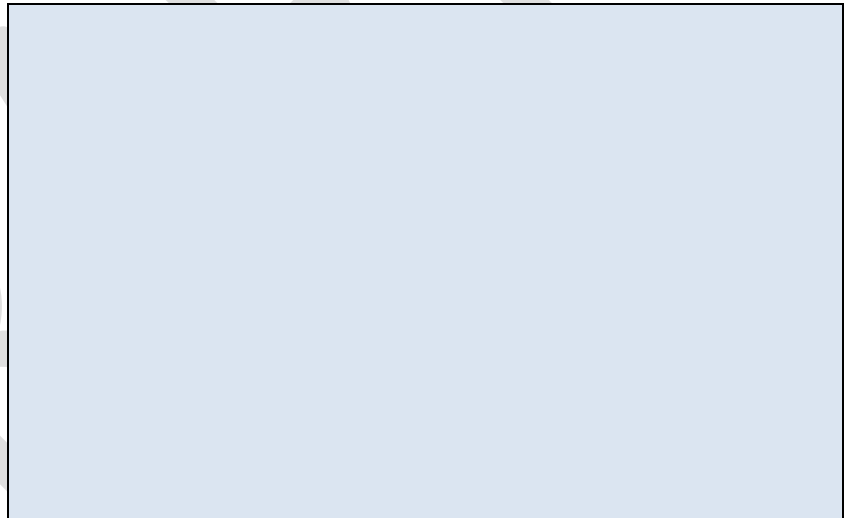
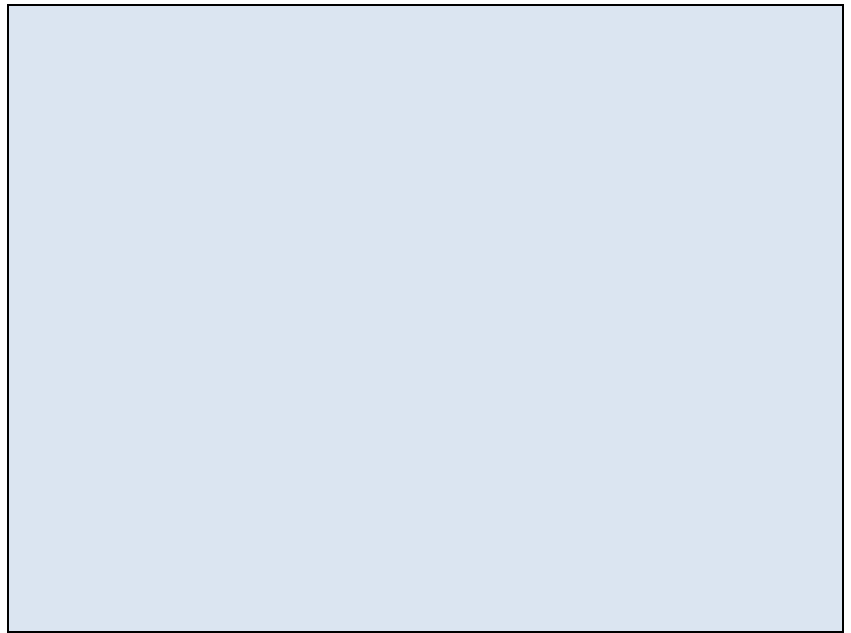
3. How will the project address stormwater management needs at this site and what additional water quality and community benefits will be provided in terms of flow and/or volume capture, flood mitigation, or aesthetic enhancement?



4. Proposed project locations should have been previously mentioned in a city/County Walk Audit or Pedestrian Bike Plan. Include the name of the document and relevant page numbers, as well as an electronic link.

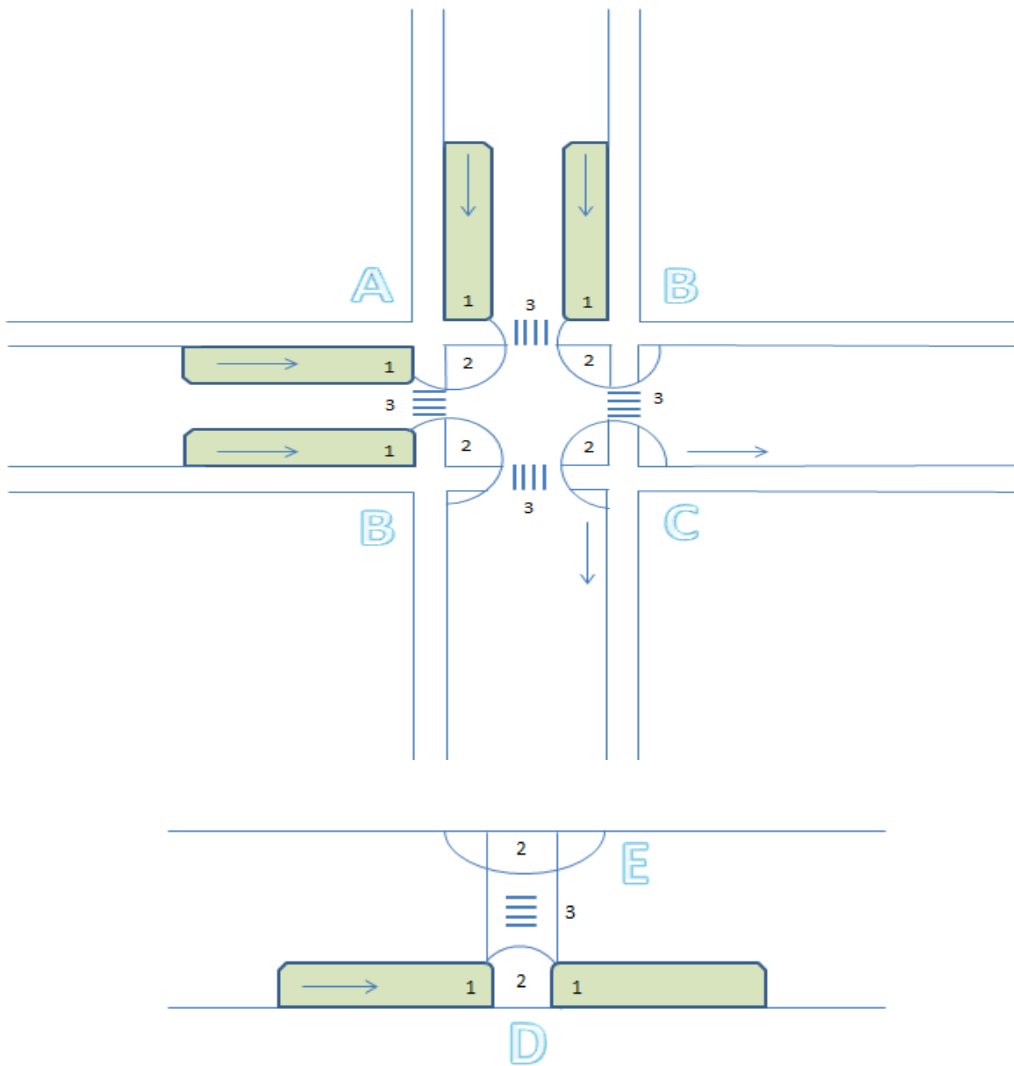
If not mentioned in Walk Audit or Pedestrian Bike Plan, proposed projects must be within a ½ mile radius of school and reasonable justification for the project should be provided, such as accident statistics or other data as to why the particular location is deemed appropriate.

5. Does the proposed project provide direct benefits to an existing local or countywide plan, or directly support goals, objectives, or projects in other plans, including bike and pedestrian master plans, community-based transportation plans, complete or sustainable streets plans, etc.? If so, provide supporting evidence of the connection between the proposed project and such plans.



Provide photos indicating existing conditions and include in your Attachments section.

Section III: Proposed Project



Project Scenarios:

A – Low point with flow from both directions toward the intersection treated by two vegetated curb extensions with a pedestrian bulbout and crossings

B – Flow from one direction going around the corner, treated with a single vegetated curb extension adjacent to a pedestrian bulbout and crossings

C – High point with flow running in both directions away from the intersection with a standard bulbout, crossings and stormwater features located elsewhere

D – Mid-block crossing with vegetated curb extensions on either or both side(s) of crossing

E – Mid-block crossing with a standard bulbout and stormwater features located elsewhere

Legend (Eligible Project Elements):

- Vegetated Curb Extensions (1)
- Pedestrian Bulbouts/Curb Ramps (2)
- Pedestrian Striping/Crosswalks (3)

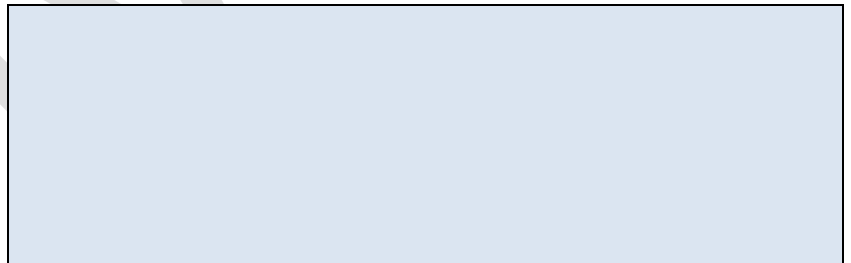
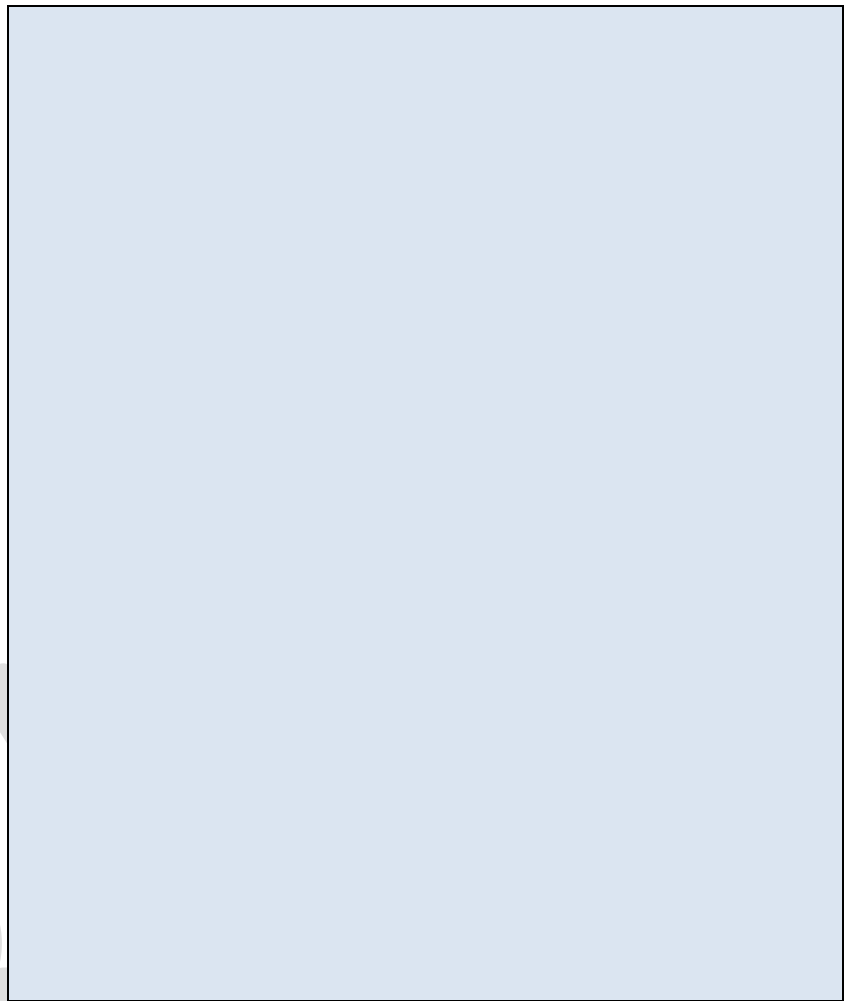
A. Project Components

1. Use the diagram on previous page to explain the proposed SRTS and stormwater infrastructure elements, demonstrating how the proposed components will achieve the SRTS and stormwater goals of this pilot program.

For example, you might describe the proposed project as a combination of scenarios A and D, where you have two vegetated curb extensions and a pedestrian bulbout at a sump condition at one corner of an intersection and a vegetated curb extension and bulbout at a mid-block crossing.

If the project includes an alternative to the general scenarios shown in the diagram above, describe the SRTS and stormwater infrastructure components, illustrating the integration of pedestrian and stormwater infrastructure and indicating the direction of stormwater flow.

2. Explain how the proposed project demonstrates a balanced approach to integrating SRTS improvements with stormwater features, both in terms of estimated costs and allocation of project features in the proposed design.



3. Include as an attachment a simple design concept of all proposed project features. Concepts should include a map delineating the drainage areas for each stormwater feature (either an estimate of the overall drainage area, including adjacent parcels, or at least the crown to curb delineation). At minimum use the 4% of drainage area sizing criteria for calculating the proposed dimensions of the stormwater features. More detailed sizing analysis via the [C.3 Technical Guidance combined flow/ volume sizing calculations](#) is encouraged to optimize sizing and assist the selection committee in evaluating projects. Where the standard C.3.d sizing criteria from the Municipal Regional Stormwater Permit cannot be met, please still include the estimated treatment capacity of the facilities for evaluation. Concepts should also show the location of existing storm drain inlets in the project area.

4. Does the project concept include educational signage to inform the public about stormwater management/SRTS goals?

B. Logistics

1. Do you have Right of Way clearance for all property involved with your project? You must confirm you have the necessary Right of Way in order to receive a grant.

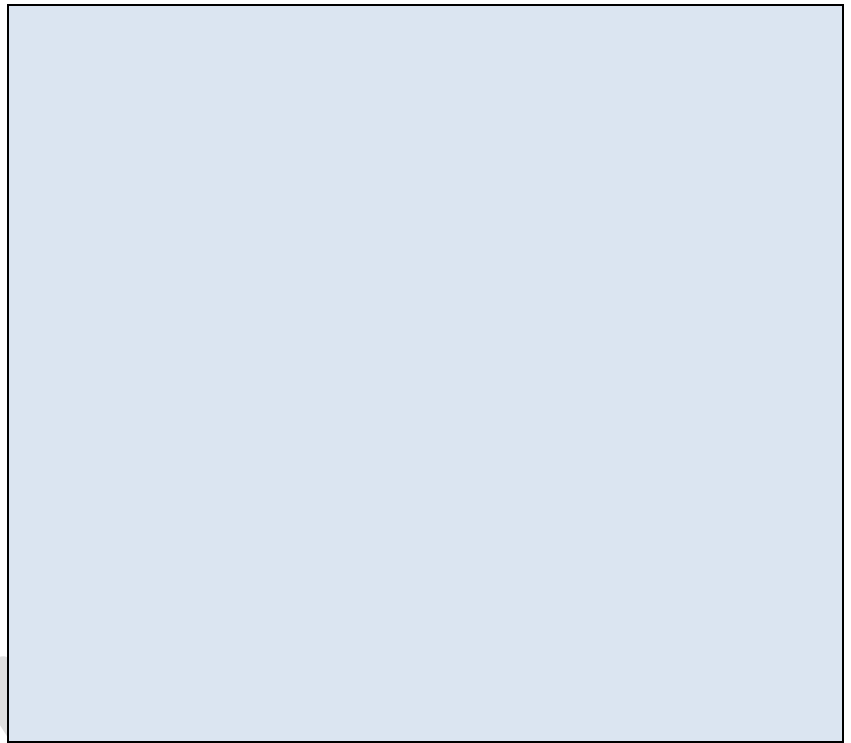
Yes

No

C. Operations and Maintenance

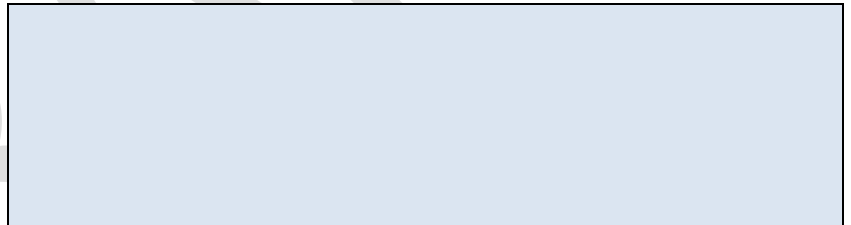
1. Provide a long-term operations and maintenance plan for the completed facilities – identify who will be responsible for long-term operations and maintenance and the dedicated source of funding to ensure sustained operations and maintenance.

Operations and maintenance plans should identify planned maintenance activities and the frequency of these activities, e.g. debris clean-out three times a year; replanting after two years of project completion (if needed), etc. See guidance document for resources. Plans may be included as a separate attachment.



D. Project Readiness

1. Indicate the Readiness to Proceed for the proposed project. Projects that demonstrate a high degree of readiness to proceed will be awarded more points in the scoring process.



Section IV: Project Timeline and Budget

A. Timeline and Budget

1. Please provide a proposed project budget and timeline, including all phases of the project (i.e., planning, design and construction). The budget should include a 15% cash only local match from the project sponsor applied to the total construction cost, so that the requested grant amount is equal to 85% of the total proposed construction cost (see application guidelines for example under Funding Details). All requested and match funds may only be used for eligible project construction costs. The proposed budget may include a 10% contingency for construction. Please include budget and timeline as an attachment to the application.

Section V: School and Community Support

A. School and Community Support

1. Does this project have the support from the participating school?

- Yes – Attach letter of support from school district

No – School district support is mandatory, grant proposals without a letter of support will not be considered

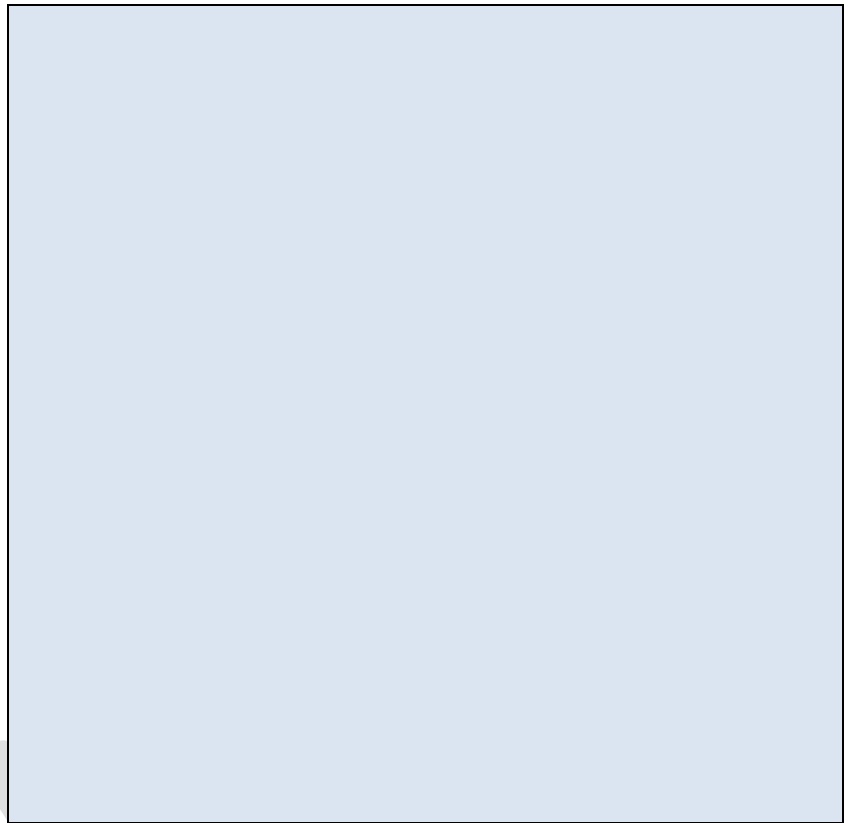
2. Does this project have local community support or involvement?

- Yes – Attach any supporting documentation (e.g. letters of support from local city council, major property owners, neighborhood associations, community groups, transit operators, etc.)

No

3. Describe any existing programs at the participating school(s) (including SRTS initiatives) that educate, encourage, or enhance walking or bicycling to school. This information can be provided by the principal of the school or SRTS coordinator and include information pertaining to any:

- Walking/biking/safety curriculum taught by the school
- Frequency of and participation in encouragement programs
- Anything else that the school/district has done that makes walking and biking easier, safer, or the preferred transportation choice



DRAFT